

AGENDA
PLANNING & ZONING MEETING
401 E. Mississippi Avenue
Tuesday, December 19, 2017
7:00 PM

CALL TO ORDER

ROLL CALL

PLEDGE TO THE FLAG

PRESENTATION OF MINUTES

1. Meeting of the Planning & Zoning Commission – October 17, 2017

PUBLIC COMMENTS

PUBLIC HEARING – *Proposed Zoning for 851 Acres Located East of Route 53 (Approximately 176 acres is annexed into the Village of Elwood and Approximately 675 acres is proposed to be annexed into the Village of Elwood) and a Special Use Permit for a Planned Unit Development*

1. Open and Conduct Public Hearing

- a) Identification of Petitioner and Interested Persons
- b) Submittal of Proof of Notice
- c) Testimony and Other Evidence by Petitioner
- d) Public Body Examination of Petitioner's Witnesses and Other Evidence
- e) Cross-examination of Petitioner's Witnesses and Other Evidence by Interested Persons
- f) Testimony and Other Evidence by Interested Persons
- g) Public Body Examination of Interested Persons' Witnesses and Other Evidence by Petitioner
- h) Cross-examination of Petitioner's Witnesses and Other Evidence by Petitioner
- i) In Some Cases, Re-examination May be Followed
- j) Report by Staff, if any
- k) Summary/Closing by Petitioner
- l) Summary/Closing by Interested Persons
- m) Rebuttal/Closing by Petitioner

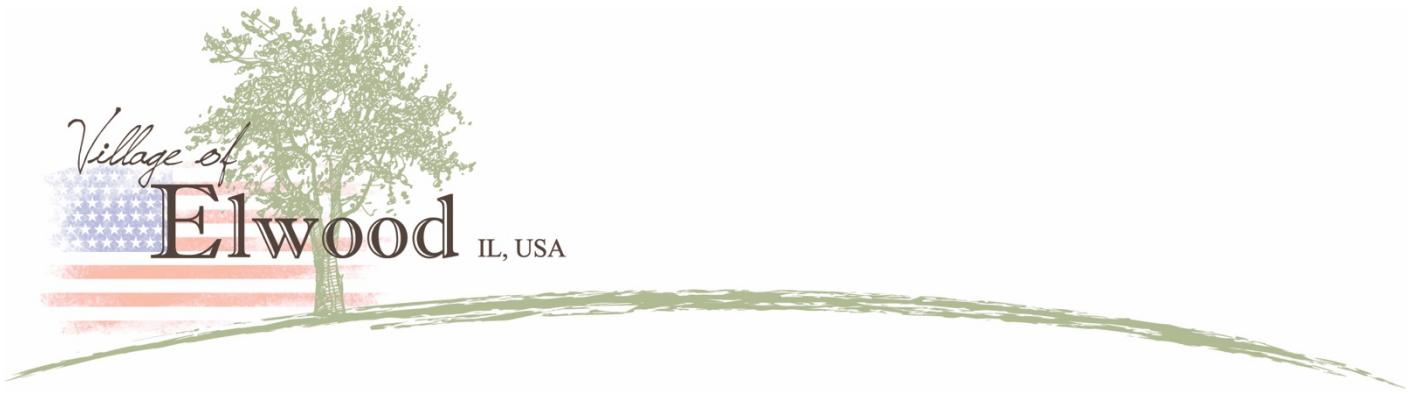
2. Close Public Hearing

3. *Actions to be Taken Following the Public Hearing*

- a) Recommend *[approval] [denial]* of the rezoning from I-4 to I-2 of the Elwood Property consisting of approximately 176 acres and the zoning upon annexation of the Property consisting of approximately 675 acres to I-2.
- b) Recommend *[approval] [denial]* of a Special Use for Planned Unit Development and a Cargo Storage Container Yard together with modifications of the Village Code as set forth in the Application.
- c) Motion to Approve the Report to the Village Board from the Plan Commission. *[Approval or Denial Report as applicable]*

OTHER BUSINESS

ADJOURNMENT



PLANNING AND ZONING COMMISSION MEETING MINUTES

Tuesday, October 17, 2017
7:00 PM

CALL TO ORDER

The Meeting of the Planning and Zoning Commission, Village of Elwood, October 17, 2017, at 7:00pm, was called to order by Chairman, Greg Hickey.

ROLL CALL

Present:

Greg Hickey, Chairman
Jake Fanning, Commissioner
Thomas W. Nagel, Commissioner
Doug Vaughn, Commissioner

Also, Present:

Julie Friebele, Village Clerk
Marian T. Gibson, Village Administrator
Jim Sparber, Village Engineer

PLEDGE TO THE FLAG

PRESENTATION OF MINUTES – Meeting of the Planning & Zoning Commission – September 26, 2017
A motion was made by Commissioner Nagel to approve the minutes of September 26, 2017, meeting as presented. Commissioner Fanning seconded the motion. All Commissioners voted in favor of the motion. The motion carried.

PUBLIC HEARING

No public hearing was conducted.

NEW BUSINESS

Village Engineer Jim Sparber presented the request made by CenterPoint properties. CenterPoint Properties has ownership of vacant property located in Lot 5 of Block 1 of Unit 6 of the CenterPoint Intermodal Center at Deer Run and requests the property be re-subdivided into three separate lots.

In addition to the creation of the aforesaid three (3) lots, CenterPoint is also proposing the creation of Lot 16 that encompasses the existing rail spur on the east side of the subdivision. In order to support the proposed lot configuration, new utility and access easements are required, and several existing easements are no longer necessary and should be vacated. The proposed use of this property remains unchanged from previously approved plans as does the infrastructure needed to serve the property. The proposed documents are in accordance with applicable Village requirements.

- *A motion was made by Commissioner Nagel to recommend to the Village Board of Trustees approval of the Plat of Easement Vacation of Lot 5 in Block 1, in CenterPoint Intermodal Center at Deer Run Unit 6. The motion was seconded by Commissioner Fanning. A vote was called: Commissioner Fanning – yes, Commissioner Nagel – yes, Commissioner Vaughn – yes. The motion carried.*
- *A motion was made by Commissioner Nagel to recommend to the Village Board of Trustees approval of the Preliminary Plat of CenterPoint Intermodal Center at Deer Run Unit 7. The motion was seconded by Commissioner Fanning. A vote was called: Commissioner Fanning – yes, Commissioner Nagel – yes, Commissioner Vaughn – yes. The motion carried.*
- *A motion was made by Commissioner Nagel to recommend to the Village Board of Trustees approval of the Final Plat of CenterPoint Intermodal Center at Deer Run Unit 7. The motion was seconded by Commissioner Fanning. A vote was called: Commissioner Fanning – yes, Commissioner Nagel – yes, Commissioner Vaughn – yes. The motion carried.*

OTHER BUSINESS

No other business was discussed.

PUBLIC COMMENTS

A member of the audience discussed the following:

NorthPoint Business Park

Road Repairs

Traffic Study – *(the traffic study was distributed to the Plan Commission)*

ADJOURNMENT

A motion was made by Commissioner Fanning to adjourn the meeting. The motion was seconded by Commissioner Nagel. All Commissioners voted in favor of the motion. The motion carried.

Julie Friebele

Village Clerk

ORDINANCE 734

AN ORDINANCE ADOPTING
RULES FOR PUBLIC HEARING
VILLAGE OF ELWOOD
WILL COUNTY, ILLINOIS

PASSED AND APPROVED BY
THE PRESIDENT AND BOARD OF TRUSTEES
THE 7 DAY OF July, 2003

Published in pamphlet form by
authority of the corporate authorities
of the Village of Elwood, Illinois,
the 9 day of July, 2003.

ORDINANCE NO. 734

**AN ORDINANCE ADOPTING
RULES FOR PUBLIC HEARING
VILLAGE OF ELWOOD
WILL COUNTY, ILLINOIS**

This Ordinance coming before the Village Board of Trustees for the Village of Elwood for consideration at its public meeting on July 7, 2003.

WHEREAS, the Village of Elwood is a Home Rule Unit under subsection (a) of Section 6 of Article XII of the Constitution of the State of Illinois of 1970; and

WHEREAS, a Home Rule Unit may exercise any power and perform any function pertaining to its government and affairs for the protection of the public health, safety, morals and welfare; and

WHEREAS, it is the desire and intention of the Village Board for the Village of Elwood to adopt standard rules for conducting administrative or quasi-judicial public hearings for the Village of Elwood in light of Klaeren v. Village of Lisle recently decided by the Illinois Supreme Court.

NOW, THEREFORE, BE IT RESOLVED BY THE BOARD OF TRUSTEES AND THE PRESIDENT FOR THE VILLAGE OF ELWOOD, ILLINOIS PURSUANT TO ITS HOME RULE AND STATUTORY AUTHORITY, AS FOLLOWS:

SECTION 1: INCORPORATION That the preambles to this Ordinance be, and the same hereby, are incorporated herein by this reference as if set out in full at this place.

SECTION 2: ADOPTION That the Rules for Public Hearing (attached hereto and incorporated herewith as Exhibit A) are hereby adopted to provide formal procedures to be utilized for all public hearings on matters of an administrative or quasi-judicial nature conducted by the Corporate Authorities for the Village of Elwood and the Elwood Plan Commission.

SECTION 3: REPEALER All Ordinances and/or Ordinances approved by the Village of Elwood which are inconsistent herewith, to the extent of such inconsistency and no further, are hereby repealed upon the effective date hereof.

SECTION 4: SEVERABILITY Should any Section, Subsection or other provision of this Ordinance for any reason be held invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not effect the validity of the Ordinance as a whole or any part not declared invalid.

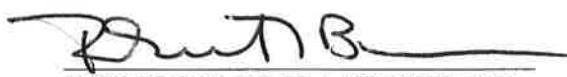
SECTION 5: EFFECTIVE DATE That this Ordinance shall be in full force and effect from and after its passage, approval, and publication in pamphlet form as provided by law.

AYES: _____ 4 _____

NAYS: _____ 0 _____

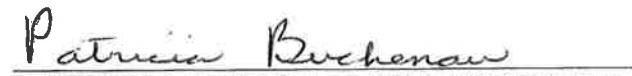
ABSENT: _____ 1 _____

PASSED this 7 day of July, 2003.



ROBERT T. BLUM, PRESIDENT

ATTEST:



PATRICIA BUCHENAU, VILLAGE CLERK

SUGGESTED RULES FOR PUBLIC HEARING

1. All hearings of the public body shall be subject to the Illinois Open Meetings Act.
2. The Chair may impose reasonable limitations on evidence or testimony presented by persons and parties barring repetitious, irrelevant or immaterial testimony. Time limits, if imposed, shall be fair, and equally administered. The public body shall not be bound by strict rules of evidence; however, irrelevant, immaterial, or unduly repetitious evidence shall not be admissible. The Chair shall rule on all questions related to the admissibility of evidence. The Chair may impose reasonable conditions on the hearing process based on the following factors:
 - a. The complexity of the issue.
 - b. Whether the witness possesses special expertise.
 - c. The degree to which the witness's testimony relates to the factors to be considered in approving or denying the proposal.
 - d. Such other factors appropriate for the hearing.
3. The Chair may take such actions as are required to maintain an orderly and civil hearing.
4. Proof of lawful notice shall be introduced into evidence before the public body.
5. A record of proceedings (i.e. a verbatim transcript prepared by a court reporter) may be made as directed by the public body.
6. At a public hearing, a Petitioner may appear on his or her own behalf or may be represented by an attorney.
7. The municipality shall be a party in every proceeding, and need not appear.
8. In addition to the Petitioner, any person may appear and participate at the hearing.
9. People participating shall identify themselves for the record, either orally or in writing, and indicate if an attorney represents them. Any person participating, other than the Petitioner, shall be referred to in these rules as Interested Person.
10. The examination of a witness shall not be used by the questioner to offer testimony or evidence of the questioner.

11. All persons offering testimony at a hearing shall testify under oath. An attorney shall be sworn if he or she offers testimony but not if he or she is questioning witnesses, summarizing testimony of witnesses, or addressing the public body.
12. The order of presentation of evidence at a public hearing shall generally be as follows, but may be modified as determined appropriate by the Chair:
 - a. Identification of Petitioner and Interested Persons.
 - b. Submittal of proof of notice.
 - c. Testimony and other evidence by Petitioner.
 - d. Public body examination of Petitioner's witnesses and other evidence.
 - e. Cross-examination of Petitioner's witnesses and other evidence by Interested Persons.
 - f. Testimony and other evidence by Interested Persons.
 - g. Public body examination of Interested Persons' witnesses and other evidence.
 - h. Cross-examination of Interested Persons' witnesses and other evidence by Petitioner.
 - i. In some cases re-examination may be allowed.
 - j. Report by staff, if any.
 - k. Summary/Closing by Petitioner.
 - l. Summary/Closing by Interested Persons.
 - m. Rebuttal/Closing by Petitioner.
13. At the conclusion of an evidentiary portion of the public hearing, the public body may, among other actions, move to deliberate its decision on the evidence presented, or continue the hearing to a date, time and location certain.
14. A written decision shall be prepared which shall include findings of fact (if required by Ordinance) and the public body's recommendation or decision based upon the record.
15. These Rules for Public Hearing may be amended by a vote of a majority of the Village Board.

Land Use Application

DISCLAIMER: Only one application per each singular petitioned use.

(Section Break)

Section 1: Petitioner Information

Please provide full contact information for the individual submitting the application.

Name	East Gate - Logistics Park Chicago, LLC
Address	<u>4825 NW 41st Street, Suite 500</u>
City	Riverside
State	MO
Zip Code	64150
Phone Number	<u>8163842292</u>
Fax Number	<i>Field not completed.</i>
Email Address	<u>imcdonald@northpointkc.com</u>
Petitioner Relationship to Request	Petitioner is the contract purchaser of the property and has attached a copy of said contract of this petition., Petitioner is acting on behalf of the owner of the subject property and has attached a letter granting such authority signed by the owner.

In the event the property is held in trust, a notarized letter from an authorized trust officer identifying the petitioner as an authorized individual acting on behalf of the beneficiaries is required. The letter must provide the name, address and percentage of interest each beneficiary holds.

(Section Break)

Section 2: Property Information

Please provide, in entirety, property information as it pertains to this application.

Address	Compass Business Park - See attached documentation
City	Elwood
State	IL
Zip Code	60421

Property Size	851 acres
PIN Number	See attached
Zoning	I-2 Light Industrial District with Special Use and PUD
Indicate which, if any, of the following supporting documents have been uploaded in the 'file upload' boxes below.	Legal Description, Adjacent Property Owners
File Upload	<u>Owners Consents for Compass Business Park.pdf</u>
File Upload	<u>List of Property Owners and Depiction of Adjacent Properties Submitted 9-12-2017.pdf</u>
File Upload	<u>Rezoning Depiction and Legal Description.pdf</u>
File Upload	<i>Field not completed.</i>

(Section Break)

Section 3: Type of Action Requested

Indicate the action requested for the aforementioned property.

Select the action requested by the petitioner.	Zoning Map Amendment
Other, please specify.	PUD with a Special Use along with Concept Plan, including recommendation for annexation, and other relief in furtherance thereof

(Section Break)

Section 4: Statement of Petition

Elaborate on the purpose of the petition and specific use as it applies to the identified property.

Statement of Petition	See attached Statement of Petition and associated application documents
File Upload	<u>Compass Business Park Application.pdf</u>
File Upload	<i>Field not completed.</i>
File Upload	<i>Field not completed.</i>

File Upload

Field not completed.

(Section Break)

Section 5: Land Use Fees

Identify the applicable fee associated with the petitioner's request.

Annexation Without
Agreement

Field not completed.

Annexation With
Agreement

Field not completed.

Zoning Map Amendment

\$1,085.00 - More than 100 acres

Variance

Field not completed.

Appeal of an Administrative
Determination

Field not completed.

Plat Application

Field not completed.

Zoning Status Report

Field not completed.

Text Amendment

Field not completed.

Hearing Recordation

Field not completed.

Site Plan Review - Choose
the Greater Amount

Field not completed.

Total Fee

\$1,085

Indicate Payment Method: Check - Mailed

Information

Please submit any additional information, attachments or payment by phone to (815) 423-5011, by fax to (815) 423-6818, by email to pat.winters@villageofelwood.com or by mail to Village of Elwood, 401 East Mississippi Avenue, Elwood, IL 60421.

(Section Break)

Section 5: Authorization and Signature

Authorization

I hereby affirm that I have full legal capacity to authorize the filing of this petition and that all of the information and exhibits herewith submitted are true and correct

to the best of my knowledge. The petitioner invites city representatives to make all reasonable inspections and investigations of the subject property during the period of processing this petition.

Signature of Petitioner Ian P McDonald

Date 12/1/2017

(Section Break)

Email not displaying correctly? [View it in your browser.](#)

Land Use Application

Village of Elwood, Illinois

PETITIONER: East Gate - Logistics Park Chicago, LLC
PHONE: 816-384-2292 (OFFICE) (HOME)
ADDRESS: 4825 NW 41st Street, Suite 500
CITY: Riverside STATE: Missouri ZIP: 64150

PETITIONER IS THE OWNER OF THE SUBJECT PROPERTY AND IS THE SIGNER OF THIS PETITION. PETITIONER IS THE CONTRACT PURCHASE OF THE SUBJECT PROPERTY AND HAS ATTACHED A COPY OF SAID CONTRACT OF THIS PETITION. PETITIONER IS ACTING ON BEHALF OF THE OWNER OF THE SUBJECT PROPERTY AND HAS ATTACHED A LETTER GRANTING SUCH AUTHORITY SIGNED BY THE OWNER

IN THE EVENT THE PROPERTY IS HELD IN TRUST, A NOTARIZED LETTER FROM AN AUTHORIZED TRUST OFFICER IDENTIFYING THE PETITIONER AS AN AUTHORIZED INDIVIDUAL ACTING ON BEHALF OF THE BENEFICIARIES AND PROVIDING THE NAME, ADDRESS, AND PERCENTAGE OF INTEREST OF EACH BENFICIARY IS ATTACHED TO THIS EXECUTED PETITION.

SUBJECT PROPERTY

LOCATION: See Attached

SIZE OF PROPERTY: Approximately 851 Acres TAX PARCEL NUMBER: See Attached
THE FOLLOWING DOCUMENTS HAVE BEEN ATTACHED:

<input checked="" type="checkbox"/> LEGAL DESCRIPTION	<input checked="" type="checkbox"/> LIST OF ADJACENT PROPERTY OWNERS	<input type="checkbox"/> PRELIMINARY PLAT
<input type="checkbox"/> PRELIMINARY PLAN	<input type="checkbox"/> IMPACT FEE FORM	<input type="checkbox"/> BANK TRUST LETTER
<input type="checkbox"/> PLAT OF SURVEY	<input type="checkbox"/> SITE PLAN	<input checked="" type="checkbox"/> FINAL PLAN

TYPE OF ACTION REQUESTED

<input type="checkbox"/> ANNEXATION	<input type="checkbox"/> PRELIMINARY PLAT/PLAN(CIRCLE ONE)	<input type="checkbox"/> CONDITIONAL USE
<input type="checkbox"/> ANNEXATION AGREEMENT	<input type="checkbox"/> FINAL PLAT/PLAN(CIRCLE ONE)	<input type="checkbox"/> VARIANCE
<input checked="" type="checkbox"/> CONCEPT PLAN	<input checked="" type="checkbox"/> MAP AMENDMENT FROM <u>I-4*</u> TO <u>I-2 w PUD</u>	<input type="checkbox"/> SITE PLAN REVIEW

I HAVE SUBMITTED THE REQUIRED FILING FEE. I UNDERSTAND THAT THE FEE IS NON-REFUNDABLE. THE FEE IS DETERMINED ACCORDING TO THE ATTACHED SCHEDULE OF FEES. I/A (INITIAL HERE) \$ 1,085 (FEE)

STATEMENT OF PETITION

PLEASE PROVIDE A BRIEF STATEMENT DESCRIBING THE PROPOSAL AS IT RELATES TO THE STANDARDS OF PETITION ACCOMPANYING THIS DOCUMENT(ATTACH ADDITIONAL SHEETS IF NECESSARY)

See Attached and Additional Documents

NUMBER OF DWELLING UNITS _____ TYPE OF UNITS _____ SQUARE FOOTAGE _____
PROPOSED TIME SCHEDULE FOR DEVELOPMEMT _____
REQUESTED VARIANCES _____

AUTHORIZATION

I HEREBY AFFIRM THAT I HAVE FULL LEGAL CAPACITY TO AUTHORIZE THE FILING OF THIS PETITION AND THAT ALL THE INFORMATION AND EXHIBITS HEREWITHE SUBMITTED ARE TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE. THE PETITIONER INVITES CITY REPRESENTATIVES TO MAKE ALL REASONABLE INSPECTIONS AND INVESTIGATION OF THE SUBJECT PROPERTY DURING THE PERIOD OF PROCESSING THIS PETITION.

STATE OF Missouri) 12/1/2017) EDDIE)
COUNTY OF Platte) DATE) SIGNATURE OF PETITIONER

COMPASS BUSINESS PARK REZONING APPLICATION

I-2 Zoning and Planned Unit Development

<u>Title</u>	<u>Pages</u>
Statement of Petition	2-6
Rezoning Depiction and Legal Description	7-11
PUD Ordinance Exhibits	12-41
<i>Development Guidelines</i>	13-18
<i>Table of Standards</i>	19-20
<i>Naturalized Detention</i>	21-22
<i>Exemplary Architectural, Site, Landscaping and Naturalized Detention Ordinance Modifications</i>	23-35
	36-41
Road Cross Sections and Plan View	42-47
<i>New Road</i>	43
<i>Primary Road</i>	44
<i>Secondary Road</i>	45
<i>Tertiary Road</i>	46
<i>Township Road</i>	47
Concept Plan	48-49
Bike & Pedestrian Paths	50-51
New Road Layout	52-53
Mississippi & 53 Improvements	54-57
Bridge Renderings	58-62
Truck Access Control Improvements	63-69
Additional Exemplary Features and Renderings	70-79

STATEMENT OF PETITION

Statement of Petition

East Gate – Logistics Park Chicago, LLC (the “Developer”) intends to develop the proposed Compass Business Park (the “Park”), which is an 851-acre real estate development primarily serving the light industrial, distribution center and warehousing markets. Approximately 176 acres of the Park (the “Elwood Property”) is located just east of Route 53 and just north of Ira Morgan St and was previously annexed into the Village of Elwood, Illinois (the “Village”). Another roughly 675 acres is intended to be annexed into the Village (the “Property”).

Formally for purposes of this application, the Developer seeks rezoning of the Park to I-2 Light Industrial District status and a Planned Unit Development for certain special uses and all other relief in furtherance of the foregoing.

As part of its application, the Developer proposes:

- Development Guidelines and a Table of Standards, subject to certain Ordinance Modifications, in order to establish specified standards for any future development of the Park;
- Exemplary architectural, site, landscaping and naturalized detention features to facilitate a known-product for all future development, including environmentally-friendly requirements and a commitment to heightened guidelines and standards for future development; and
- A special use for a portion of the Property, which would be primarily or exclusively used for cargo container storage up to three containers high (a “Container Storage Facility”); provided, the total amount of Property utilized for a Container Storage Facility shall not exceed 33 acres but shall not be less than 20 acres, the Container Storage Facility shall not be located on the Elwood Property, the Container Storage Facility shall be located on one parcel, the Container Storage Facility shall not be located contiguous to a Primary Road, unless otherwise agreed to by the Village, and the Container Storage Facility shall be subject to a payment of lieu of taxes in the amount of \$0.35 per square foot of gross land area (or the equivalent of \$15,246 per acre) of such Container Storage Facility.

While approximately 42 acres of the Elwood Property is intended for imminent development as a distribution center, the remaining portion of the Elwood Property would be held back for future light manufacturing uses. In addition, the vast majority of the Park will be located principally over 1 mile east of Route 53 and would be primarily utilized for distribution center and warehouse facilities.

The focal point of Compass Business Park is to create a restricted-access semi-truck road network (the “Truck Network”) using a system of overhead barriers and semi-truck turn-arounds to limit the ability of semi-trucks to access the Park from local or residential roads, including Route 53. And, instead, focus semi-truck related activity within the Park to Ira Morgan Street and then to a bridge over Route 53. The bridge would serve as a “flyover”, connecting Ira Morgan St and Walter Strawn while preventing any and all access to Route 53. The purpose of the bridge is to create a dedicated route to the local Union Pacific and BNSF intermodal facilities, and limit associated

highway traffic to the state-upgraded intersection of Arsenal and Interstate 55. A depiction of this dedicated truck route is set forth below:



Employee traffic would be allowed to enter and exit the proposed Compass Business Park via all existing and new roads. As mentioned above, semi-trucks will be prevented from entering or exiting Compass Park via these roads by a system of overhead barriers and associated semi-truck turn-arounds, as depicted in the application materials, requiring such vehicles to utilize the Truck Network.

As part of the Park development, significant public infrastructure improvements are contemplated. These include the construction of a water tower and additional well facility, at no direct cost to the Village. This also includes the construction of an additional ten to fifteen miles of on-site and off-site road improvements, the construction of an approximately 400-foot flyover bridge, as mentioned above, at an estimated cost of \$20 million. If authorized by the Illinois Department of Transportation, the Developer would also pay for the installation of a traffic signal at Mississippi Road and Route 53.

As part of the above public improvements, the Developer would provide an allowance for future landscaping, site and trail improvements to adjacent parks in the Village. And as part of its plan for the bridge construction and public improvements, the Developer will provide connectivity between the existing Village paths west of Route 53 and Midewin and other paths east of Route 53. As part of this plan, the Developer proposes to include a segregated pedestrian path on the bridge over Route 53. The path would be located on the north side of the bridge and approaches, and would then cross under the road via a tunnel with landscaped approaches. After crossing through the tunnel, the path would extend to Hoff Road and then to the corner of Hoff Road and Route 53. To provide connectivity to the eastern edge of the Village, and as an amenity for employees working in Compass Business Park, the Developer will extend a 10' path along all primary roads through Compass Business Park.

Through the Planned Unit Development, including heightened Development Guidelines and Table of Standards, the Developer is focused on creating a pleasing aesthetic experience for future employees within Compass Business Park, including those travelling through, from or to Compass Business Park. To that end, the Developer proposed thoughtful and heightened setback requirements (relative to the current Village code). These setback requirements increase in the event trailer parking is placed in a yard adjacent to a roadway. In addition, the Developer has proposed a requirement for "wrought-iron look" aluminum fence when adjacent to roadways, with specific setbacks for fencing that is equivalent to the setback for paving.

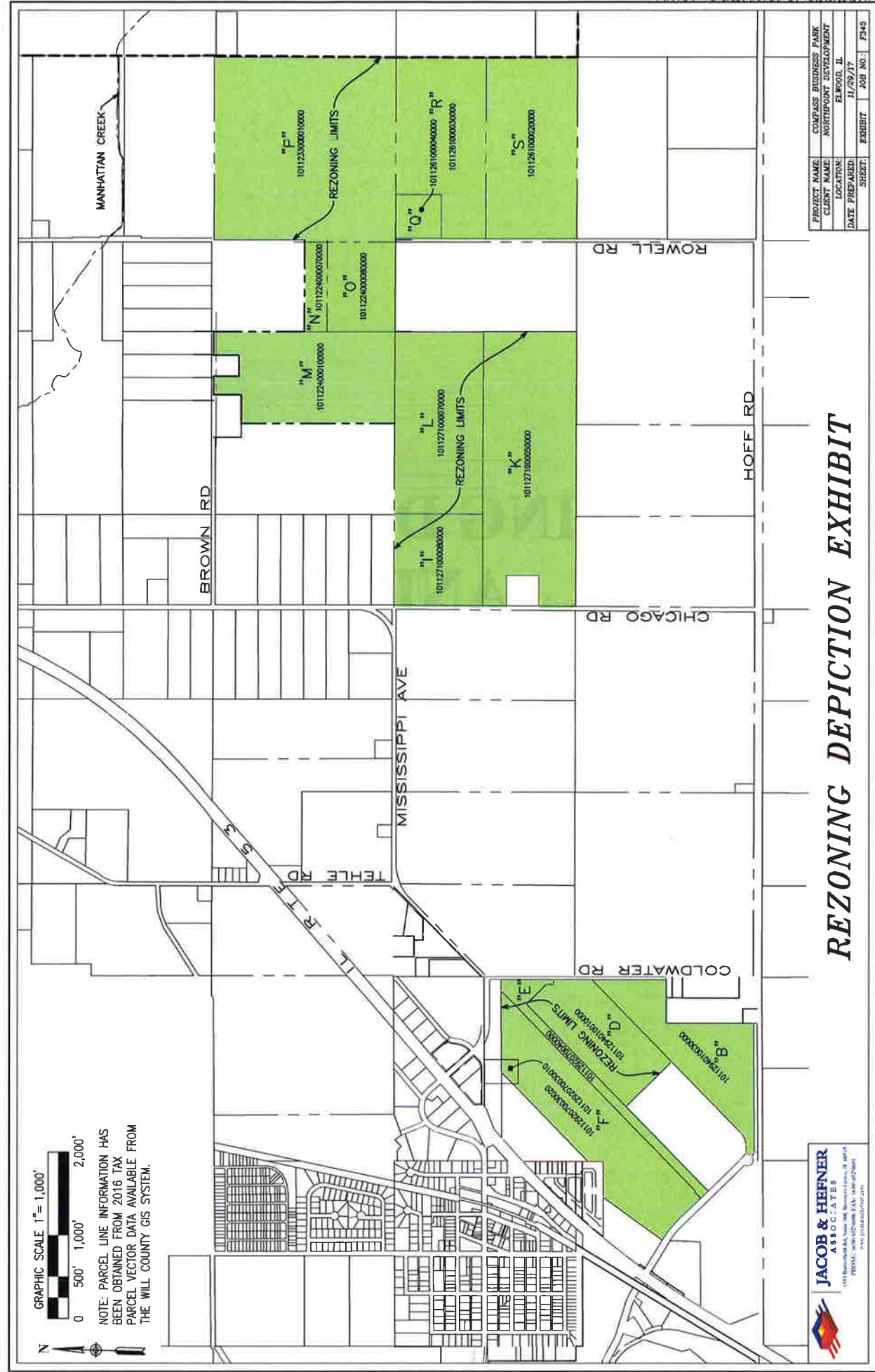
Consistent with this aesthetic requirement and to maintain an environmentally conscience appeal, future development of Compass Business Park is subject to a requirement for one tree per twenty feet on each side of each primary road. To limit visual impacts, berms will be utilized to help screen views; tree islands will be placed in parking lots; foundation yards will be placed between car parking and buildings, which will include trees and bushes; and there will be a requirement for trees to be placed on the building site equivalent in number to one tree per forty linear feet of side and rear yards. This is in addition to those required along street-facing yards. The trees will be selected from the list of native species approved for use in the Village, along with the attached list specifically selected for noise attenuation as set forth in the Development Guidelines and Exemplary Architectural, Site, Landscaping and Naturalized Detention exhibit.

The Developer is proposing the exclusive use of naturalized detention facilities throughout the 675-acre portion of the Property. The naturalized detention will be planted with the native species from the list included within the proposed Planned Unit Development. Typical detention facilities require chemical treatment and pollutants while providing little filtration of those chemicals and pollutants. Typical detention facilities also offer very little groundwater and aquifer recharge. In contrast, naturalized stormwater detention offers increased infiltration of groundwater and aquifer recharge, providing increased sustainability. Naturalized detention also filters chemicals and pollutants, increases wildlife habitation for local and migratory animals, prevents erosion while increasing soil stability, and reduces stormwater runoff volume, resulting in stormwater rate reduction.

In addition, the Developer is also sensitive to the impact of development on the night sky and will, therefore, require all lighting to be “Dark Sky” compliant, and will prohibit the external lighting of buildings or signs.

All of the above, and more, is a significant portion of the proposed application for rezoning of the Property to I-2 Light Industrial District status and a Planned Unit Development for certain special uses and all other in furtherance of the foregoing.

REZONING DEPICTION AND LEGAL DESCRIPTION



REZONING DEPICTION EXHIBIT

A graphic scale bar consisting of a horizontal line with a break in the middle. The left section is divided into four equal segments, with '0' at the start and '500' at the 1/4 mark. The right section is divided into four equal segments, with '1,000' at the 1/2 mark and '2,000' at the 3/4 mark. Above the scale bar, the text 'GRAPHIC SCALE 1" = 1,000' is written vertically.

NOTE: PARCEL LINE INFORMATION HAS BEEN OBTAINED FROM 2016 TAX PARCEL VECTOR DATA AVAILABLE FROM THE WILL COUNTY GIS SYSTEM.



Rezoning Legal Description

TRACT "B" (P.I.N. 10-11-29-401-003-0000)

ALL OF LOT 4 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS.

TRACT "D" (P.I.N. 10-11-29-401-001-0000)

ALL OF LOT 5 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS.

TRACT "E" (P.I.N. 10-11-29-207-004-0000)

ALL OF LOT 2 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS.

TRACT "F" (P.I.N. 10-11-29-207-003-0000)

ALL OF LOT 6 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS.

TRACT "I" (P.I.N. 10-11-27-100-008-0000)

THE WEST 1347.98 FEET OF THE NORTHWEST QUARTER, EXCEPT THE SOUTH 1343.14 FEET THEREOF, IN SECTION 27, IN TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

TRACT "K" (P.I.N. 10-11-27-100-005-0000)

THE SOUTHERN 1343.14 FEET OF THE WEST HALF OF THE NORTHEAST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS. ALSO, THE SOUTHERN 1343.14 FEET OF THE NORTHWEST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS, EXCEPTING THEREFROM THE NORTH 466.70 FEET OF THE SOUTH 996.70 FEET OF THE WEST 466.70 FEET THEREOF.

TRACT "L" (P.I.N. 10-11-27-100-007-0000)

THE WEST HALF OF THE NORTHEAST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS, EXCEPT THE SOUTHERN 1343.14 FEET THEREOF. ALSO, THE NORTHWEST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS, EXCEPT THE SOUTHERN 1343.14 FEET THEREOF AND ALSO EXCEPTING THE WESTERN 1347.98 FEET THEREOF.

TRACT "M" (P.I.N. 10-11-22-400-010-0000)

THE SOUTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS. ALSO, THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, EXCEPTING THEREFROM THE NORTH 400 FEET OF THE WEST 415 FEET THEREOF AND ALSO EXCEPTING THAT PART OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS COMMENCING AT THE NORTHEAST CORNER OF SAID SOUTHEAST QUARTER; THENCE WEST, ALONG THE NORTH LINE OF SAID SOUTHEAST QUARTER 1664.12 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING WEST, ALONG SAID NORTH LINE 300.00 FEET; THENCE SOUTH, PERPENDICULAR TO THE LAST DESCRIBED COURSE, 363.00 FEET; THENCE EAST, PERPENDICULAR TO THE LAST DESCRIBED COURSE, 300.00 FEET; THENCE NORTH, PERPENDICULAR TO THE LAST DESCRIBED COURSE, 363.00 FEET, TO THE POINT OF BEGINNING, IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

TRACT "N" (P.I.N. 10-11-22-400-007-0000)

THE NORTH 10 ACRES OF THE SOUTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

TRACT "O" (P.I.N. 10-11-22-400-008-0000)

THE SOUTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22 IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, EXCEPT THE NORTH 10 ACRES THEREOF, IN WILL COUNTY, ILLINOIS.

TRACT "P" (P.I.N. 10-11-23-300-001-0000)

THE SOUTHWEST QUARTER OF SECTION 23, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

PUD ORDINANCE EXHIBITS

DEVELOPMENT GUIDELINES

Development Guidelines

Any improvements, modifications, additions or demolitions of any kind performed on the Property must be designed and constructed according to the following:

Exterior Lighting:	<p><u>Site Lighting:</u> Site lighting shall be designed and installed to provide a safe, functional, attractive, and coordinated exterior environment and to minimize the amount of glare visible from, and light spillage onto neighboring properties.</p> <p>All exterior fixtures shall be mounted on the building or on clear anodized, tapered, aluminum poles. Poles shall not exceed 35' in height. Fixtures shall be prefinished aluminum and shall use high pressure sodium bulbs with a maximum wattage of 400, or, alternatively, LED bulbs with a similar light output, and shall meet LEED, Green Globe, or "Dark Sky" criteria for eliminating up-light. Fixtures mounted on buildings shall not exceed 35' in height.</p> <p>Fixtures used near exit doors shall also meet LEED, Green Globe, or "Dark Sky" criteria for eliminating up-light and shall use high pressure sodium bulbs with a maximum wattage of 150, or, alternatively, LED bulbs with a similar light output.</p> <p>Bollards shall be prefinished aluminum and shall use bulbs with a maximum wattage of 60 watts.</p> <p>Due to concerns regarding unnecessary light pollution external lighting of ground mounted monument signs, flood lighting of building mounted signs, ground mounted building accent lighting, up-lighting of landscaping, and excessive security lighting are prohibited.</p> <p>Ground or building mounted sign lighting shall be accomplished by backlighting and/or internal lighting.</p>
Equipment Screening	<p>All ground mounted mechanical or electrical equipment and all office roof top mechanical equipment visible from the adjacent road(s) shall be screened from view. Equipment required to be screened shall be screened utilizing one or more of the following methods:</p> <ul style="list-style-type: none">• Landscaping• Architectural metal panels mounted horizontally, with a baked-on enamel finish to match the building color• EIFS designed to match the building panels• Precast• Parapet walls• Opaque glass

	<ul style="list-style-type: none"> Proprietary integrated equipment screens may be approved by Village staff provided the number of separate screens in one location is two or less. <p>Great care should be taken to locate pipes, flues and hoods out of view from adjacent roads.</p> <p>Rooftop radio, TV, microwave, and other antennas must be specifically approved by Village staff as to their height, location, and screening.</p>
Site Paving:	<p><u>Entrance Aprons:</u> Entrance aprons shall include a depressed curb and gutter (or a curb and gutter which has had the back of the curb removed via sawing) and at least fifteen (15) feet of concrete.</p> <p><u>Asphalt Pavement:</u> All areas designed exclusively for automobile traffic will be paved with no less than three (3) inches of asphalt (in two lifts) placed over no less than eight (8) inches of compacted CA-6. All areas designed for truck traffic will be paved with no less than four (4) inches of asphalt (in two lifts) placed over no less than ten (10) inches of compacted CA-6. All asphalt paved areas shall be graded to slope at least 1.0% for drainage purposes. A 6" nonreinforced Ductilcrete slab installed over a 4" aggregate base may be substituted for heavy duty paving and the truck apron (required below).</p> <p><u>Truck Docks:</u> All facilities shall be designed and constructed to provide adequate maneuvering on-site without the need to maneuver on the street to access any truck docks located on that site. All exterior truck docks shall have a concrete apron at least seventy (70) feet adjacent to the dock wall.</p> <p><u>Curbing:</u> All paved areas (asphalt and concrete) shall be bounded by a concrete curb. All landscape areas shall be separated from paved areas with a concrete curb. Where bioswales are used, breaks in curbs shall be allowed to collect and route water into swales.</p>
Landscaping:	<p><u>General:</u> The purpose of the landscaping requirements is to provide a visually attractive, functional and coordinated environment that is reasonable to maintain and relatively tolerant or resistant to disease. Each site shall, at a minimum, conform to all requirements of the Village ordinances, except where amended herein.</p> <p><u>Irrigation:</u> Lawn areas adjacent to the right-of-way shall be irrigated to the back of curb. It is strongly recommended that individual lot owners consider the use of an alternative water supply for a portion of the site irrigation needs.</p> <p><u>Berms:</u> Earth berms shall be incorporated wherever practical. Berms should undulate in height and should generally not exceed a slope of four to one with maximum slope three to one when needed.</p>

Lawn Areas: All areas which are not improved with building or paving, or planted with landscape material shall be seeded or sodded. Ornamental stone may be used.

Parking Lot Islands: Automobile parking lots are to include a tree island, 9' x 18' in size for every twenty (20) parking spots. Automobile parking shall not be placed directly against any building, but shall respect the "Foundation Yards" provided in the Table of Standards. Each row of automobile parking shall terminate in a tree island.

Retaining Wall/Terracing: Where it is necessary to utilize retaining walls to transition grade, no individual wall shall exceed six (6) feet in height. If a transition of greater height is required multiple walls shall be used and terraced with a minimum horizontal offset between walls of six (6) feet. Walls shall be built using integrally colored split face concrete modular units.

Tree and Shrub List: All trees and shrubs planned to be installed adjacent to public roadways or in parking lots must be from the Village's list of approved parkway trees. The following list of native shrubs and trees are included due to their "sound buffering" capabilities and their use is encouraged whenever not prohibited by Village ordinance:

Shrubs:

1. Dogwood species
2. Witch Hazel
3. Elderberry
4. Black Chokeberry
5. Black Haw

Trees:

1. Eastern White Pine
2. Eastern Red Cedar
3. Redbud
4. Red Maple
5. Oak Species
6. Hackberry
7. Arborvitae
8. Black Walnut
9. Sycamore

<p>Building Architecture/ Materials</p>	<p>The purpose of the Development Guidelines is to produce an orderly and aesthetically pleasing development of high quality architecture in harmony with the environment, consistent with the theme of the business park and consistent with the intended use of the buildings.</p> <p>Design compatibility should be provided throughout the site. An overall continuity can be achieved by use of similar or compatible materials, colors and textures. Within developments comprised of two or more structures, heights, massing and form of articulation should be of similar character.</p> <p>Buildings should be responsive to the Illinois climate and indigenous landscape. Buildings should incorporate sun shading and other devices to effectively temper the area's climate. Insulation and thermal glazing should meet energy code standards. Colors used on buildings should complement the business park's development scheme. Buildings shall exhibit a comprehensive campus-like environment through the use on all buildings of similar signage, exterior lighting components and landscape materials.</p> <p><u>Elevations:</u> All elevations of a structure shall be constructed of the same material. This is not to require that glass features or other entry area highlights be added to the back of a building. This is, however, to require that if the front elevation of a building is constructed with smooth precast with cast-in reveals, then the other three elevations shall be constructed with smooth precast with cast-in reveals (except as provided below for walls enclosing a freezer/cool space or a wall specifically designed as an expansion wall). Main entrances to the buildings shall be well defined. Service doors shall be integrated into the overall design of the building. Stairs or elevators required for multi-story facilities shall be contained inside the facilities</p> <p><u>Materials:</u> All precast or site cast concrete wall panels shall have a smooth exterior finish with cast-in reveals. All concrete wall panels shall be stained or painted.</p> <p><u>Highlights:</u> The following materials shall be allowed as highlight material for entry areas or other architecturally significant areas of a structure:</p> <ul style="list-style-type: none"> • EIFS • Standing Seam Metal • Architectural Metal Panels • Glass • Block/Brick/Stone (brick shall be utility size and of high quality and of low moisture absorbing). Block shall be allowed only as a highlight base material and shall be of a high quality and low moisture absorbing and shall have a smooth or rough-cut limestone appearance <p><u>Prohibited Materials:</u> the following materials are prohibited on the exterior</p>
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	<p>structure unless otherwise approved by Village staff:</p> <ul style="list-style-type: none"> • "Ribbed" or "raked" precast panels • "Double T" precast panels • EIFS in any location lower than five (5) feet above grade. • Standard concrete block. • Wood • Ceramic Tile • Quarry Tile • Metal wall panels (other than the exterior wall of a freezer/cool space within a structure, or a wall specifically designed as an expansion wall) • Asphalt shingles.
<p>Outdoor Storage (Excluding Container Storage)</p>	<p><u>General:</u> It is required that all activity such as processing, manufacturing, or assembling take place within a fully enclosed building. Where outdoor storage is allowed it will be located in such a way to minimize its view from any adjacent roadway. Outdoor storage shall be limited to twelve (12) feet in height and shall be screened from view as follows:</p> <ul style="list-style-type: none"> • From adjacent roads - Storage to be completely screened by wall panels matching the building panels; or, berthing and landscaping. • From adjacent property lines - Storage to be fifty (50) percent screened by wall panels matching the building panels; or, berthing and landscaping.
<p>Fencing</p>	<p><u>General:</u> Where it is necessary to use fencing the following requirements shall apply:</p> <ul style="list-style-type: none"> • Front Yards/Corner side yards - Fencing shall maintain the setbacks require in the Table of Standards. Fence material shall be aluminum (wrought iron look) and shall be powder coated black. Maximum height shall be twelve (12) feet. • Side/Rear Yards - Fencing shall be located on the property line. Fence material shall be aluminum (wrought iron look) powder coated black, or chain link with black vinyl coating. Maximum height shall be twelve (12) feet.

TABLE OF STANDARDS

Table of Standards

Setbacks										Foundation Yard (N/A in Truck courts)		Tree Spacing (Each Side)									
Feature		Road Width (BxB)		R-O-W Width		Roadside Easements		Naturalized Detention		Car Parking		Fencing		Trailer Storage Curb		Track Maneuvering Curb		Building			
Roadway																					
Primary**	40	66	20' Both Sides	40	40	40	40	40	40	40	40	40	40	40	40	40	62	10	20' (Staggered)		
Secondary	40	42	20' Both Sides*	35	35	35	35	35	35	47.5	35	35	35	35	35	35	62	10	40		
Tertiary	40	42	20' Both Sides	30	30	30	30	30	30	42.5	30	30	30	30	30	30	62	10	40		
Setbacks										Naturalized Detention		Car Parking		Fencing		Trailer Storage Curb		Track Maneuvering Curb		Building	
Yard																					
Interior Side																					
Side Abutting Residential																					
Rear																					
Rear Abutting Residential																					
Parking Tree Island/Curb Cuts																					
Miscellaneous:																					
One car and drive aisle																					
Two cars and drive aisle																					
Four cars and drive aisle																					
Parking stall width																					
Tree Island Requirement																					
Tree Island Size																					
Minimum curb cut separation																					

*May be placed in other yards
**Subject to consultation with neighbor
*** A 10' easement on one side of the ROW shall be permitted within the "New Road" located on the Elwood Property

4' Bx8
6' Bx8
12' Bx8
9'
Every 20 cars
Same as Car Stall
25' (at straight)

NATURALIZED DETENTION

Naturalized Detention

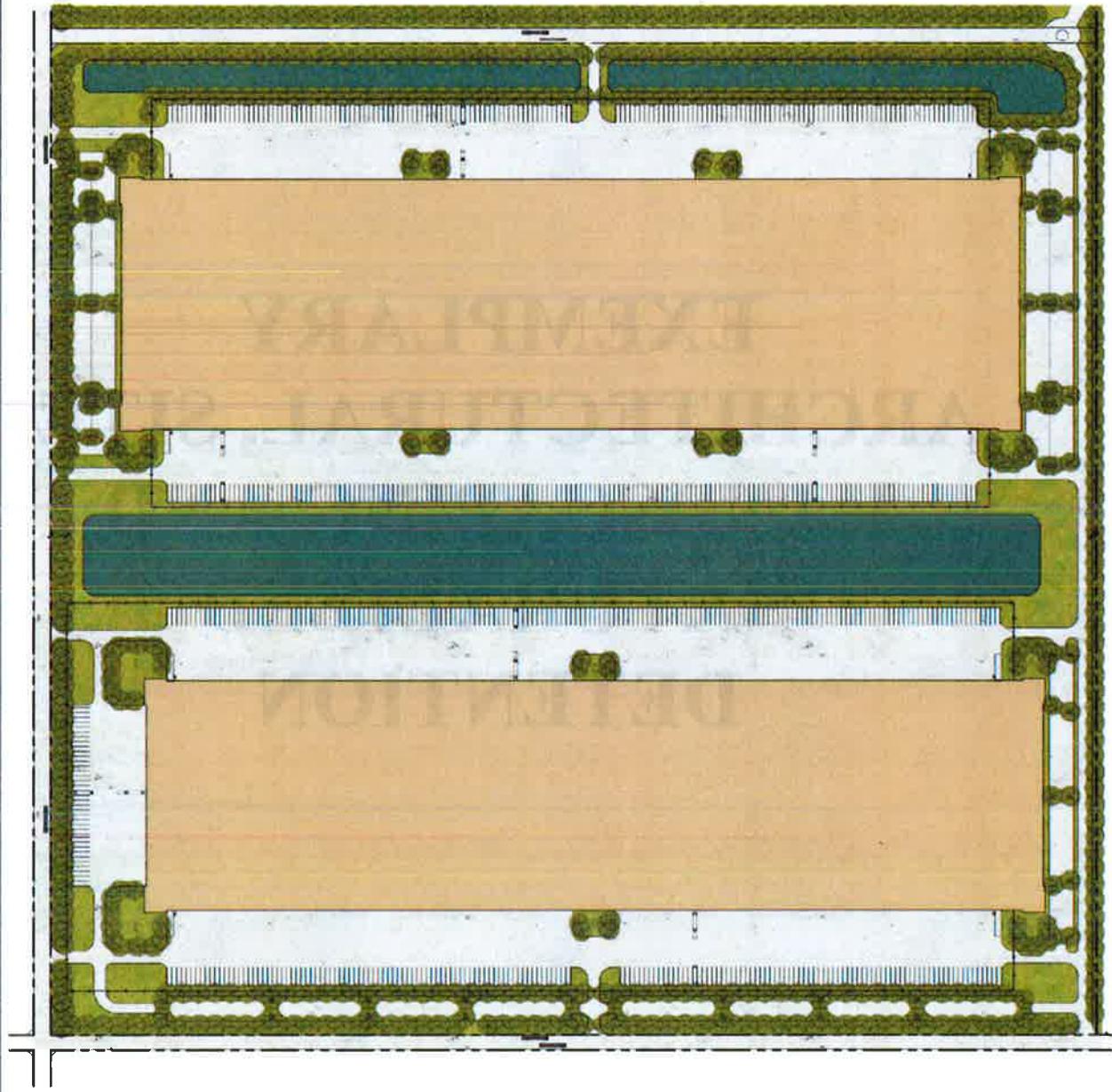
All new detention facilities constructed on the Property shall be naturalized facilities. Naturalized detention facilities are to be planted sides and bottoms with appropriate prairie and wetland plantings to:

- Increase infiltration of groundwater and aquifer recharge
- Reduce volume and rate of stormwater runoff beyond typical detention facilities
- Filter pollutants
- Reduce soil erosion
- Provide habitat

The following native plants are to be used exclusively for the Naturalized Detention areas:

1. Compass Plant
2. Canada Wild Rye
3. Little Bluestem
4. Black Eyed Susan
5. Switch Grass
6. Ohio Spiderwort
7. Prairie Cord Grass
8. Rice Cut Grass
9. Dark Blue Bulrush
10. American Sweet Flag
11. Broadleaf Arrowhead

**EXEMPLARY
ARCHITECTURAL, SITE,
LANDSCAPING AND
NATURALIZED
DETENTION**





EXEMPLARY ARCHITECTURAL, SITE, LANDSCAPING AND NATURALIZED DETENTION

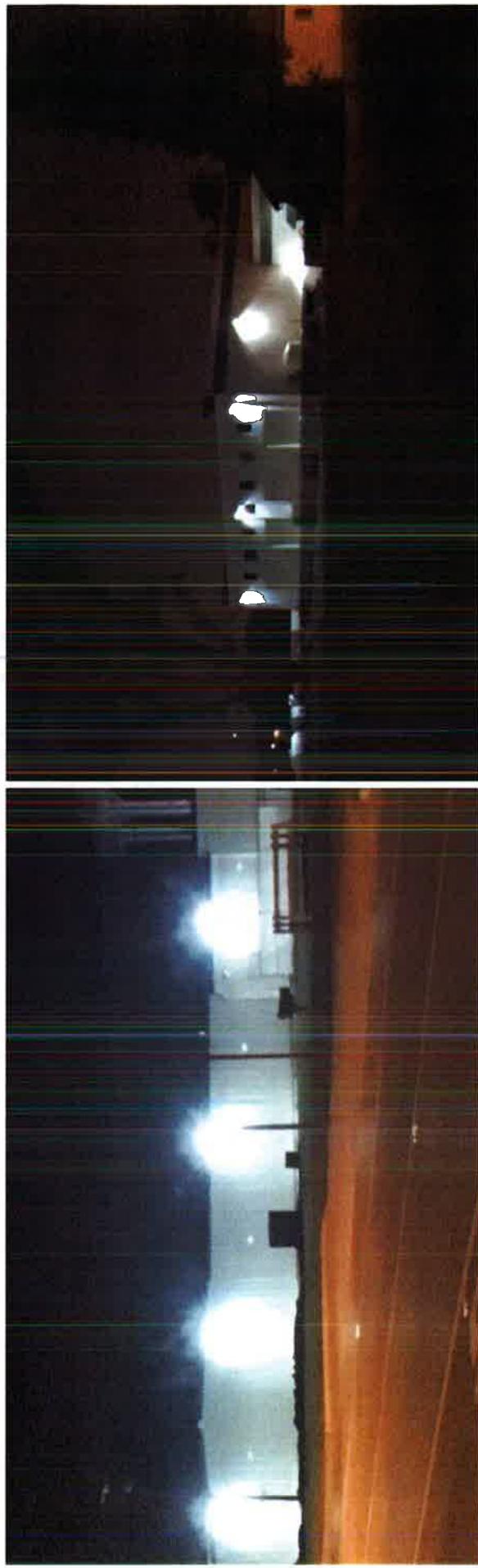


SAMPLE BUILDING LAYOUT

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EXEMPLARY ARCHITECTURAL, SITE, LANDSCAPING AND NATURALIZED DETENTION



NOT Dark Sky Compliant

Dark Sky Compliant
Compass Business Park

DARK SKY COMPLIANT LIGHTING



EXEMPLARY ARCHITECTURAL, SITE, LANDSCAPING AND NATURALIZED DETENTION



LANDSCAPED ISLAND





EXEMPLARY ARCHITECTURAL, SITE, LANDSCAPING AND NATURALIZED DETENTION



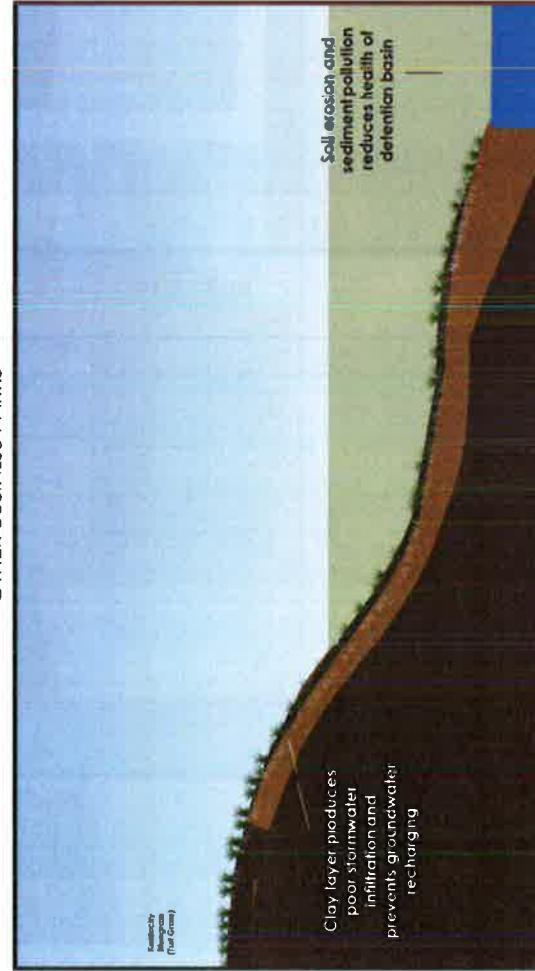
DOUBLE ROW LANDSCAPING





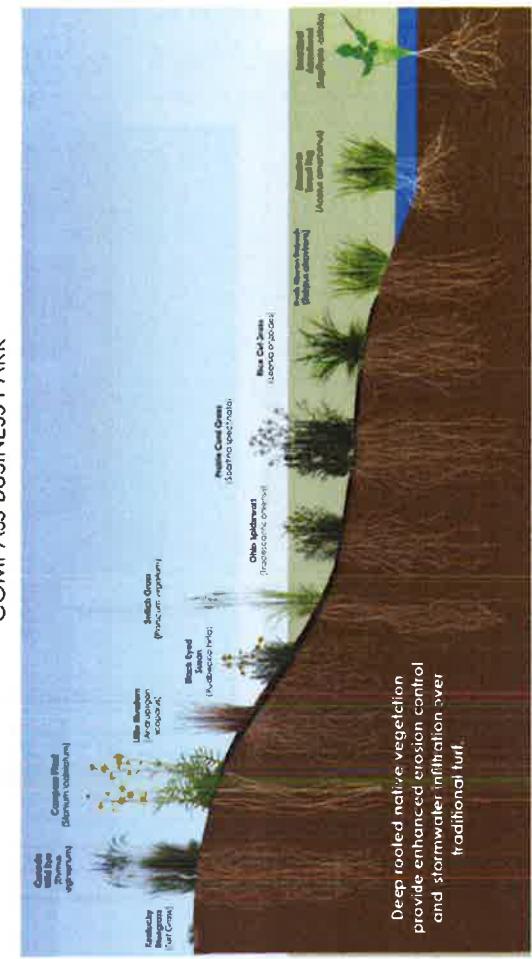
EXEMPLARY ARCHITECTURAL, SITE, LANDSCAPING AND NATURALIZED DETENTION

TURF GRASS DETENTION BASIN OTHER BUSINESS PARKS



ENCAP
INCORPORATED

NATURALIZED DETENTION BASIN COMPASS BUSINESS PARK



ENCAP
INCORPORATED

Typical Detention



Naturalized Detention



Typical Detention



Turf Grass Ponds - Severe shoreline erosion

Naturalized Detention



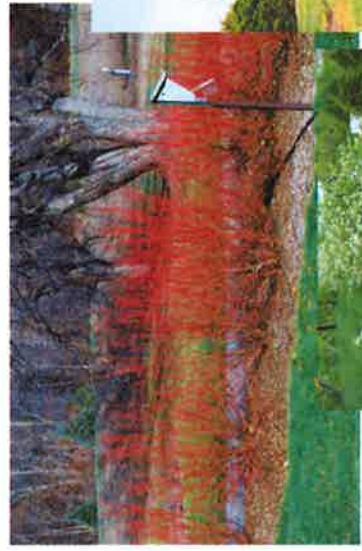
Mature Naturalized Detention

EXEMPLARY ARCHITECTURAL, SITE, LANDSCAPING AND NATURALIZED DETENTION

Native Tree/shrub List – Sound Buffering

Shrubs

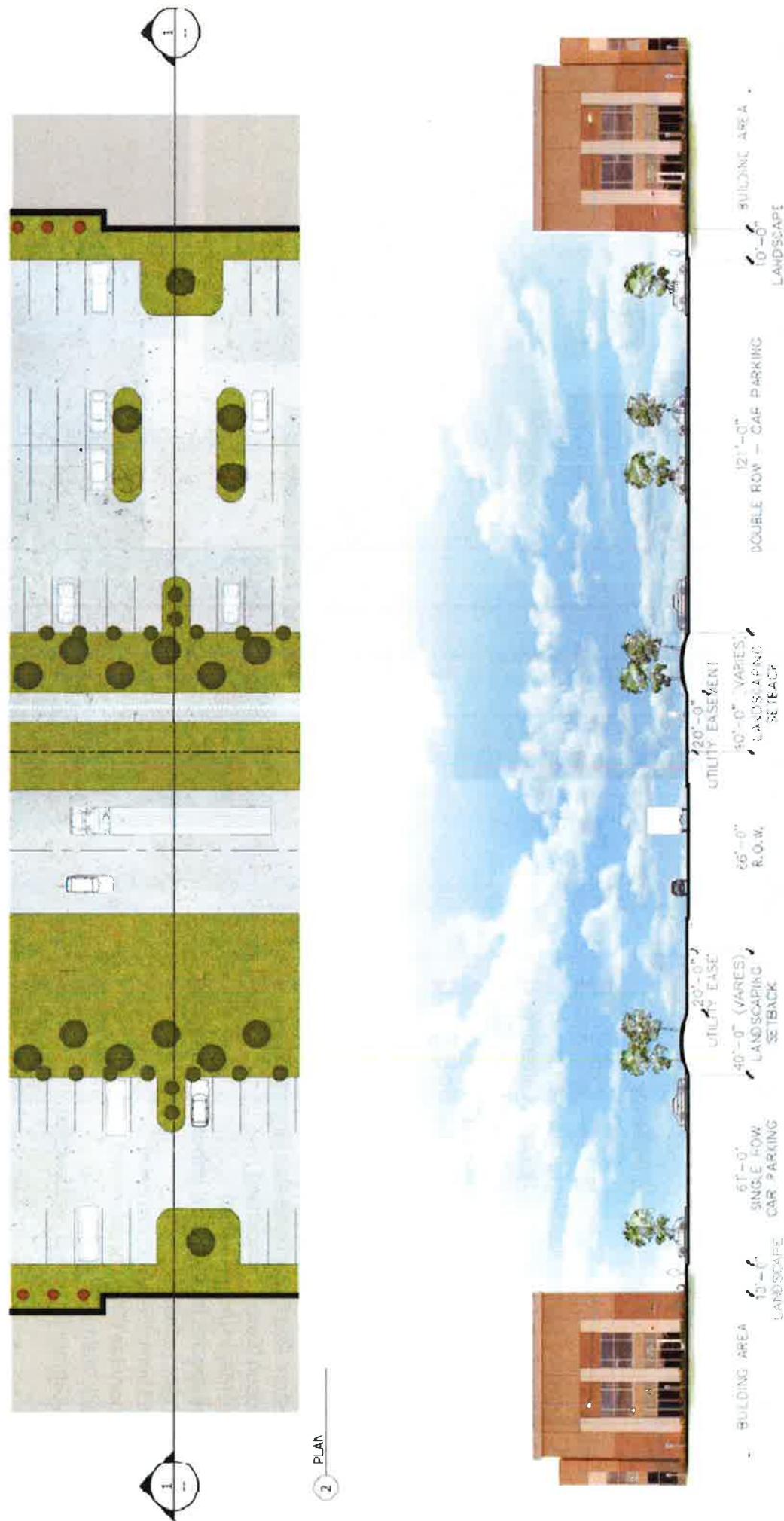
1. Dogwood species (*Cornus racemosa*, *Cornus stolonifera*, *Cornus obliqua*, *Cornus alternifolia*)
2. Witch Hazel (*Hamamelis virginiana*)
3. Elderberry (*Sambucus canadensis*)
4. Black Chokeberry (*Aronia melanocarpa*)
5. Black Haw (*Viburnum prunifolium*)



Trees

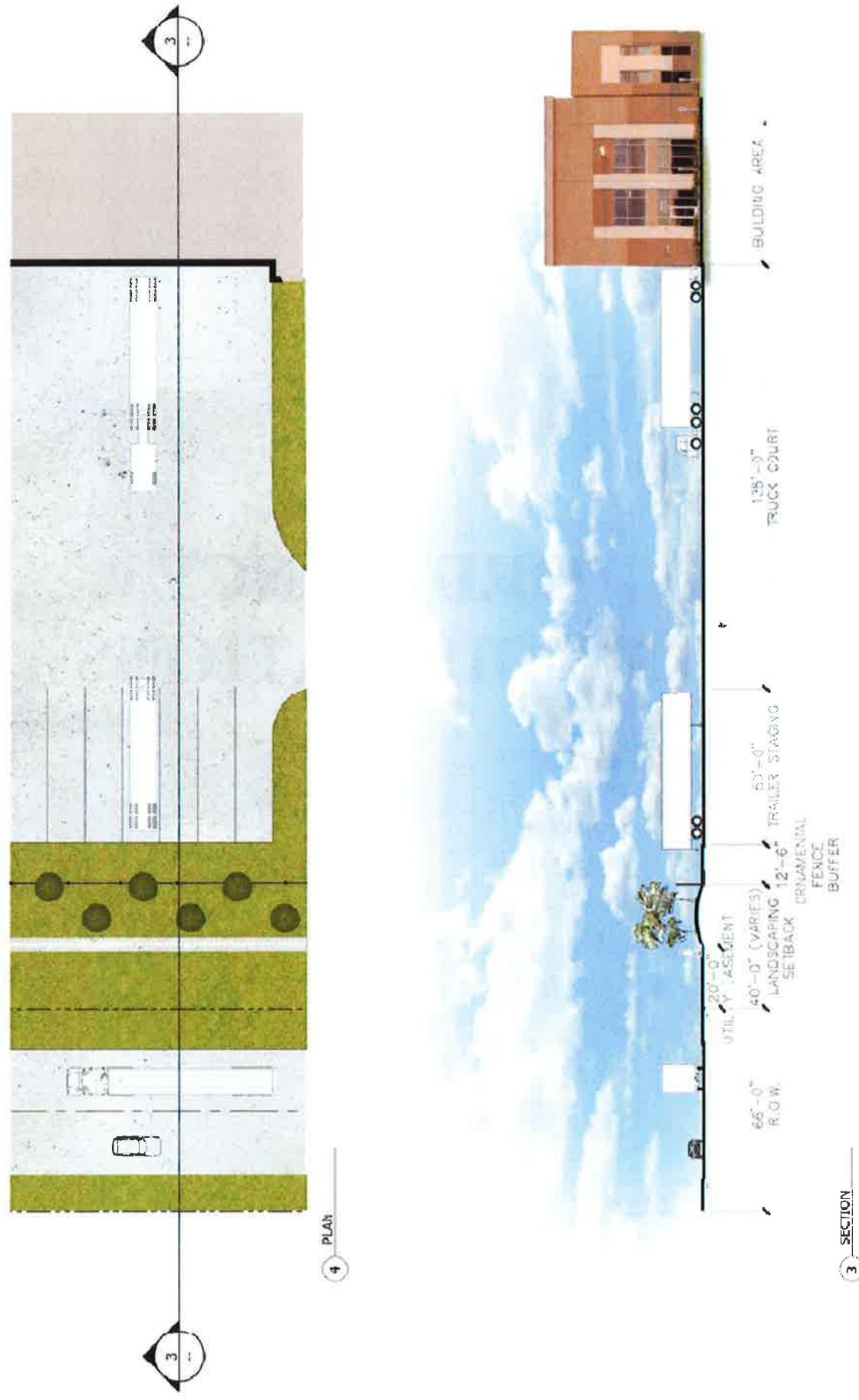
1. River Birch (*Betula nigra*)
2. Eastern White Pine (*Pinus strobus*)
3. Eastern Red Cedar (*Juniperus virginiana*)
4. Redbud (*Cercis canadensis*)
5. Red Maple (*Acer rubrum*)
6. Oak species (*macrocarpa*, *imbricaria*, *rubra*, *alba*, *bicolor*, *palustris*)
7. Hackberry (*Celtis occidentalis*)
8. Arborvitae (*Thuja occidentalis*)
9. Black Walnut (*Juglans nigra*)
10. Sycamore (*Platanus occidentalis*)





COMPASS
BUSINESS PARK

EXEMPLARY ARCHITECTURAL, SITE, LANDSCAPING AND NATURALIZED DETENTION



ORDINANCE MODIFICATIONS

Ordinance Modifications

Note: In all cases if an item listed below appears in another section of the Village's codes or ordinances it shall likewise be modified.

150.02 – Limitation for Construction Activity (A): Limitations on the hours of construction activity set forth in the Village Codes and Ordinances shall only apply to the Park where construction activity is conducted within one thousand feet (1,000') of an occupied residential structure, which may be subject to modification and limitations in the event of construction activity for public improvements.

150.06 – Cargo Container Storage Facilities, up to three containers high, will be an allowed use in the Park; provided, the total amount of the Park utilized for a Container Storage Facility shall not exceed 33 acres but shall not be less than 20 acres, the Container Storage Facility shall not be located on the Elwood Property, the Container Storage Facility shall be located on one parcel, the Container Storage Facility shall not be located contiguous to a Primary Road, unless otherwise agreed to by the Village, and the Container Storage Facility shall be subject to a payment of lieu of taxes in the amount of \$0.35 per square foot of gross land area (or the equivalent of \$15,246 per acre) of such Container Storage Facility.

151.02 – Industrial Impact

(A)(1): Given the financial benefits to the Village, and donation to the Village of retail zoned land, and donation to the Village of land for a public works facility, the requirement to set-aside 10% of the Park for public use, or, in lieu there-of, make a payment to the Village equal to 10% of the acreage of the Park times \$20,000 per acre will not be applicable to the Park. No donation or set-aside of land for public use, or payment in lieu of, will be due for the Park, other than as otherwise agreed to by the parties.

(B)(2): Given the benefits accruing to local public bodies from the proposed development of the Park, including the recreational, park enhancements, and Learning & Career Center, the requirement to donate land, or cash in lieu of, for schools, parks, libraries, or any other use will not be applicable to the Park. No donation of land, or payment in lieu of, will be due for schools, parks, libraries, or any other use, for the Park, other than as otherwise provided by mutual agreement.

153 – Building Code; and Ordinance 1043 – An Ordinance Establishing a Uniform Building Code

1. The Village agrees, and the Village has confirmed the Elwood Fire District agrees, that for any building equipped with an ESFR fire protection system, and equipped with mechanical smoke venting capable of one air change per hour via roof-mounted exhaust fans and roof-mounted gravity-operated intake hoods, the Village and Elwood Fire District will approve an increase in the exit travel distance from 250' to 400' without the need for a smoke study. It is further agreed that the mechanical smoke venting system may be used by the building occupant for fresh air ventilation provided the appropriate controls are included.

2. The Village agrees, and the Village has confirmed the Elwood Fire District agrees, that for any building equipped with an ESFR fire protection system the Village and the Elwood Fire District will require the location of fireman entry doors only where required to provide internal exiting provided each exit door is equipped with a fireman's hose drop.
3. The Village agrees, and the Village has confirmed the Elwood Fire District agrees, that for any building equipped with an ESFR system the Village and the Elwood Fire District will not require a fire detection system in the warehouse area.
4. The Village agrees that permits for any facility on the Park will be issued in phases. The Village will accept and review the plans for each phase of work, and will issue comments, or permits, within fifteen (15) business days following submittal of the necessary information for the permit phase sought. The phases may include, but are not limited to the following:
 - Grading and excavating;
 - Underground utilities;
 - Building foundations;
 - Building shell;
 - Fire sprinkler;
 - Shell electrical, HVAC, and plumbing;
 - Interior improvements;
 - Tenant racking, equipment, furniture, and fixtures;
 - Occupancy.
5. The Village and the Elwood Fire District agree, that fire protection systems do not have to be "zoned" by tenant.

156 – Stream and Wetland Protection

156.05 and 156.07: Provided Owner complies with all laws and regulations of the applicable agency(ies) with jurisdictional authority over stream and wetland protection and modification, including, but not limited to the Army Corps of Engineers, FEMA, IDOT, and IEPA, these sections of the village code will not be applicable to the Park. The Village hereby agrees that, other than the Manhattan Creek, Owner may relocate any streams or watercourses on the Park, including into underground pipes, provided Owner obtains approval from all applicable agencies.

161 – Subdivision

161.023 and 161.024 – Procedure for Preliminary and Final Plat Approval: A clarification that preliminary and final plats for subdivisions of the Park may proceed contemporaneously.

161.025 – Procedure for Site Plan Approval: Because of the comprehensive nature of the Park's intended development and PUD, and due to the comprehensive codes and ordinances of the Village, review of the site plan for any building proposed to be constructed on a subdivided parcel, on a parcel which has been submitted to the Village for subdivision, or on a parcel exempt from

subdivision requirements will be performed by Village staff for compliance with the Village's codes and ordinances, as modified herein and by mutual agreement. Therefore, the site plan for such building will not be required to be submitted to, or reviewed by the Plan Commission or the Village Board. If Village staff finds the site plan for such building complies, staff will, upon submittal and subsequent approval of the appropriate plans, as mentioned above, grant permits for all phases of construction, mentioned above, except that "occupancy" permits will not be issued prior to subdivision of the subject parcel, unless the subject parcel is exempt from subdivision requirements.

161.048(B)(3) and 161.054(B)(3) – Performance and Maintenance Guarantee: The Village agrees that bonds are a form of financial assurance acceptable to the Village.

161.065 – Prerequisites for Issuance of Building Permits

(A) This requirement is modified in 161.025 above.

(B) This requirement is waived other than the requirement for suitable access for emergency vehicles. Such access can be provided via a stone construction road properly maintained by Owner.

(B)(2): The reference to "30 days" is modified to "90 days" and the word "reasonable" shall be inserted prior to the word "discretion".

161.105 – Preservation of Natural Features: Manhattan Creek is a natural feature within the Park.

161.111 – Private Utilities – Existing, overhead utilities in or serving the Park will not be required to be placed or relocated underground. All new utilities at the Park will be located underground.

161.112 – Site Plan Requirements (includes architectural requirements)

(A)(2)(a): This paragraph of the Village code is modified to require that Owner must provide ornamental/architectural features on building walls that face a public street along the lesser of (i) 20% of the total length of the wall facing such public street, or, (ii) 160 feet.

(A)(2)(c): The Village agrees that loading docks and trailer storage will be allowed in front of buildings on the Park, and in the area abutting public and private streets provided a green space is maintained between the back of curb of the public or private street and the back of curb of the trailer storage pavement area, or the truck dock maneuvering pavement area per the Table of Standards. Fences installed in the aforementioned green space shall be ornamental and shall be located per the Table of Standards. All other fences at the Park may be vinyl coated chain link.

(A)(2)(d): A clarification that any equipment placed on a building roof for heating, ventilating, or air conditioning of the warehouse or manufacturing area need not be screened provided it is located at least 60' back from the nearest edge of the building.

(B)(3): A clarification, correction, or modification that Owner shall provide for any building on the Park one approved planting for each 5,000 square feet of gross lot area.

(E) Access: In order to facilitate the separation of passenger vehicles and trucks, the requirement for 75' separation for curb cuts will be reduced to 25' for the Park per the Table of Standards. The limitation of two curb cuts per site will not be applicable to the Park. The limitation of 40' width for curb cuts will be increased to 48' (as measured at the narrowest point) for the Park, with the ability to increase the width in situations where truck turning requirements dictate.

Appendix B – Car Parking Dimensions: Car stalls on the Park will measure 9' in width and 18' in length to the face of an adjoining curb or the line separating an adjacent car stall. Drive aisles will measure 24'. One landscape island will be provided for every twenty cars in car parking lots on the Park. The landscape islands will have an inside dimension of 9' in width and will be planted with one tree.

162 – Zoning

General

162.069 – I-2 Light Industrial District

(F) Density and Dimensional Requirements. See Table of Standards.

162.069(F)(4) – Clarification that the maximum impervious coverage on each parcel shall be 87.5%.

162.069(F)(5) – Building Height Limitations: Building heights up to 65' will not require a special use permit. Front Yard and Corner Side Yard setbacks required by zoning, as amended by mutual agreement, will be increased by one foot for each two feet of building height in excess of forty-five feet (45'). Building height will be measured from finished floor to the mid-height of the sloped roof. Chimneys, cooling towers, elevator head houses, storage silos, tanks, water towers, roof-mounted HVAC equipment, antennas, and other appurtenant equipment usually required to be placed above the roof level are not subject to the increased setback provision herein prescribed.

162.151 – Location of Off-Street Parking and Loading

(D)(2) It is expressly acknowledged and agreed by the Village that car parking and truck maneuvering areas may serve adjacent lots as long as the total number of required car parking stalls and loading berths for each building are provided. Shared car parking stalls and/or truck maneuvering pavements may cross shared property lines and, in such cases, no setback or green space requirement shall be applicable.

162.177 – Signs Permitted in Industrial Zoning Districts

(E)(2) Wall Signs: The maximum area of wall signs within the Park will be 200 square feet per sign.

(E)(3) Number of Wall Signs: A business located within the Park will be allowed two wall signs if the signs are located on two different elevations, or on the same elevation but at least 400' apart.

Village of Elwood Engineering Technical Standards dated April 19, 2006

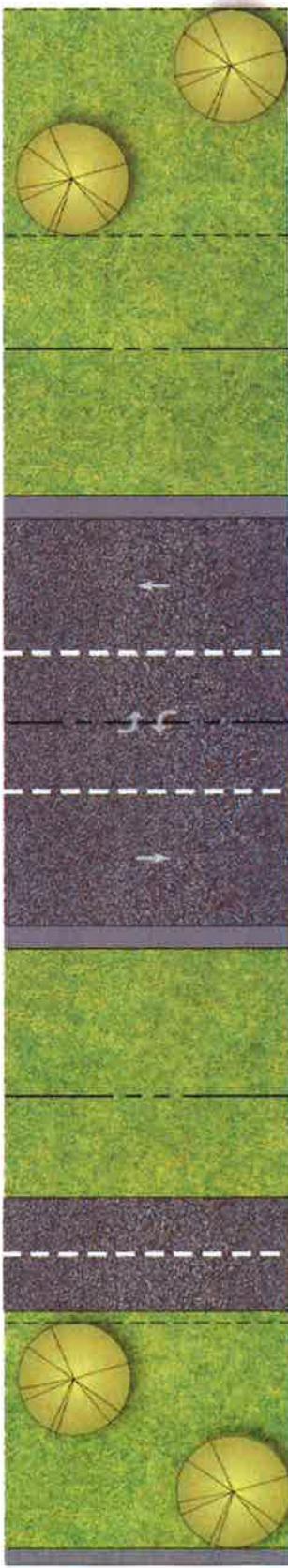
Section 2.02(A)(2): The parties acknowledge that the traffic study contemplate all required street improvements, so for purposes of clarification, there will be no requirement for Owner to improve roads adjacent to the Park, unless otherwise agreed to by the parties.

Section 3.02(E)(1): A clarification that temporary dead-end watermains may be over 100' in length.

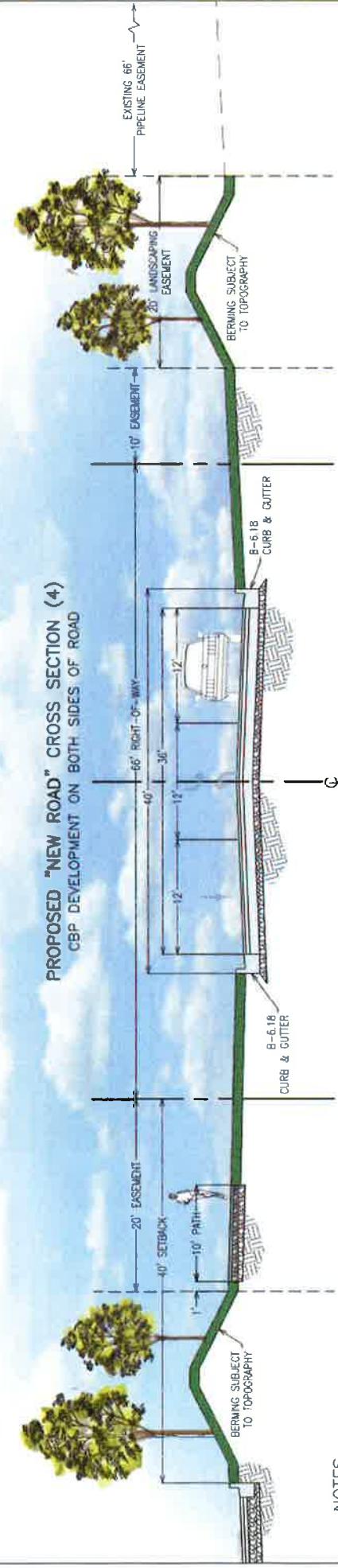
Section 4.04(F)(1): A clarification that sanitary sewer service connections to the main need not be "wye" connections.

Section 5.03(B)(1): A clarification that side slopes of all detention facilities may be 3:1, and that the maximum allowable difference between "normal" water level and "high" water level may be 10'.

ROAD CROSS SECTIONS AND PLAN VIEW



PROPOSED "NEW ROAD" CROSS SECTION (4)
CBP DEVELOPMENT ON BOTH SIDES OF ROAD



NOTES

1. IT SHALL BE PERMISSIBLE TO UTILIZE CONCRETE PAVEMENT OF SIMILAR STRUCTURAL VALUE IN LIEU OF HOT-MAK-ASPHALT (HMA) PAVEMENT.
2. IT SHALL BE PERMISSIBLE TO ADJUST THE SPECIFICATIONS AND LIFT THICKNESSES OF THE PROPOSED PAVEMENT IN ORDER TO PROVIDE A COMPARABLE OR SUPERIOR FINISHED PRODUCT (E.G. INSTALL 2" HMA SURFACE COURSE AND 7" BIT. BASE IN LIEU OF 1.5" HMA SURFACE AND 7.5" BIT. BASE).

PUBLIC ROADWAY PAVEMENT SECTION

1.75" BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", NS0
2.5" BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, NS0
7.5" BITUMINOUS BASE COURSE, SUPERPAVE, NS0
4" CRUSHED AGGREGATE SURFACE GRANULAR MATERIALS, TYPE B

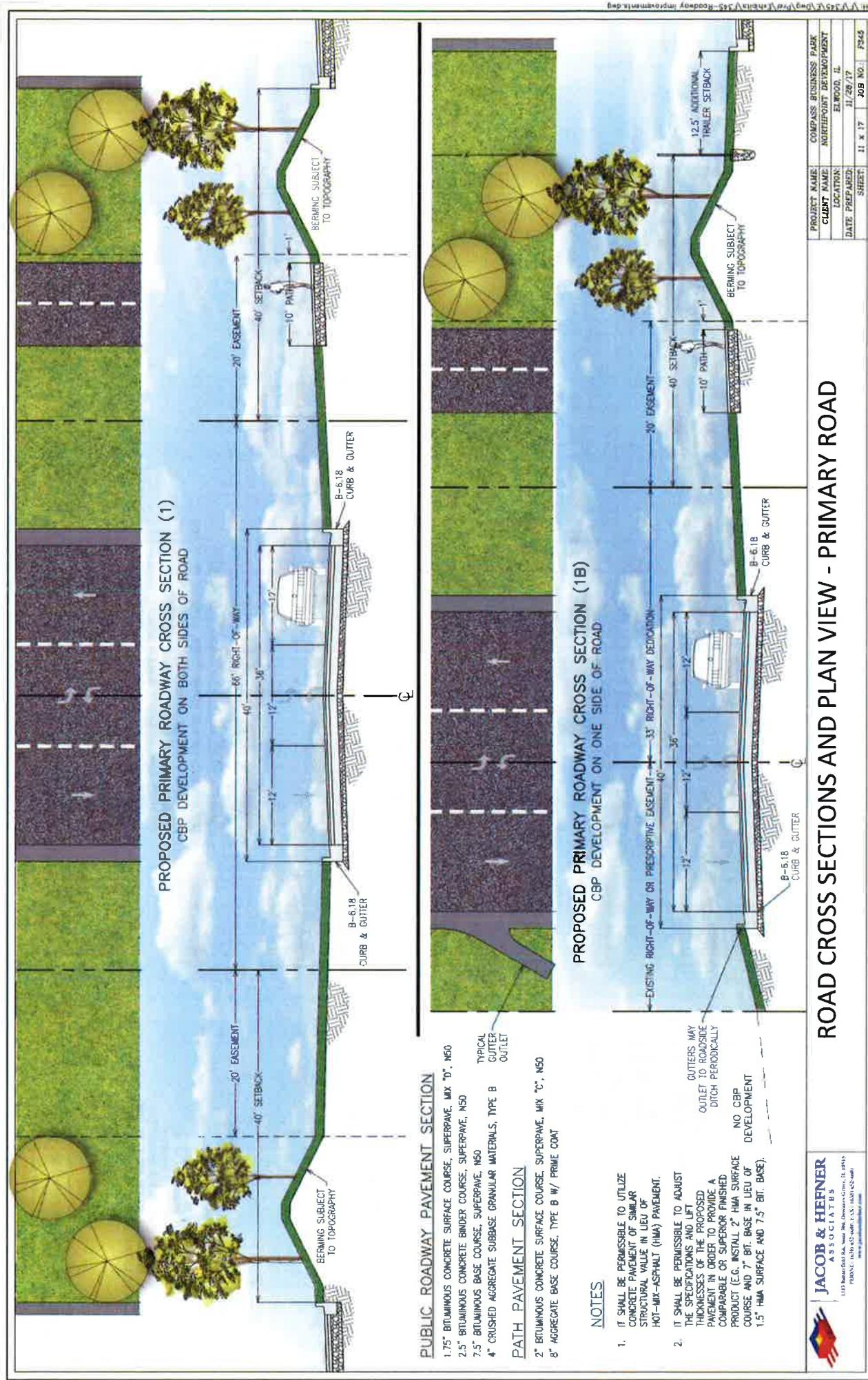
PATH PAVEMENT SECTION

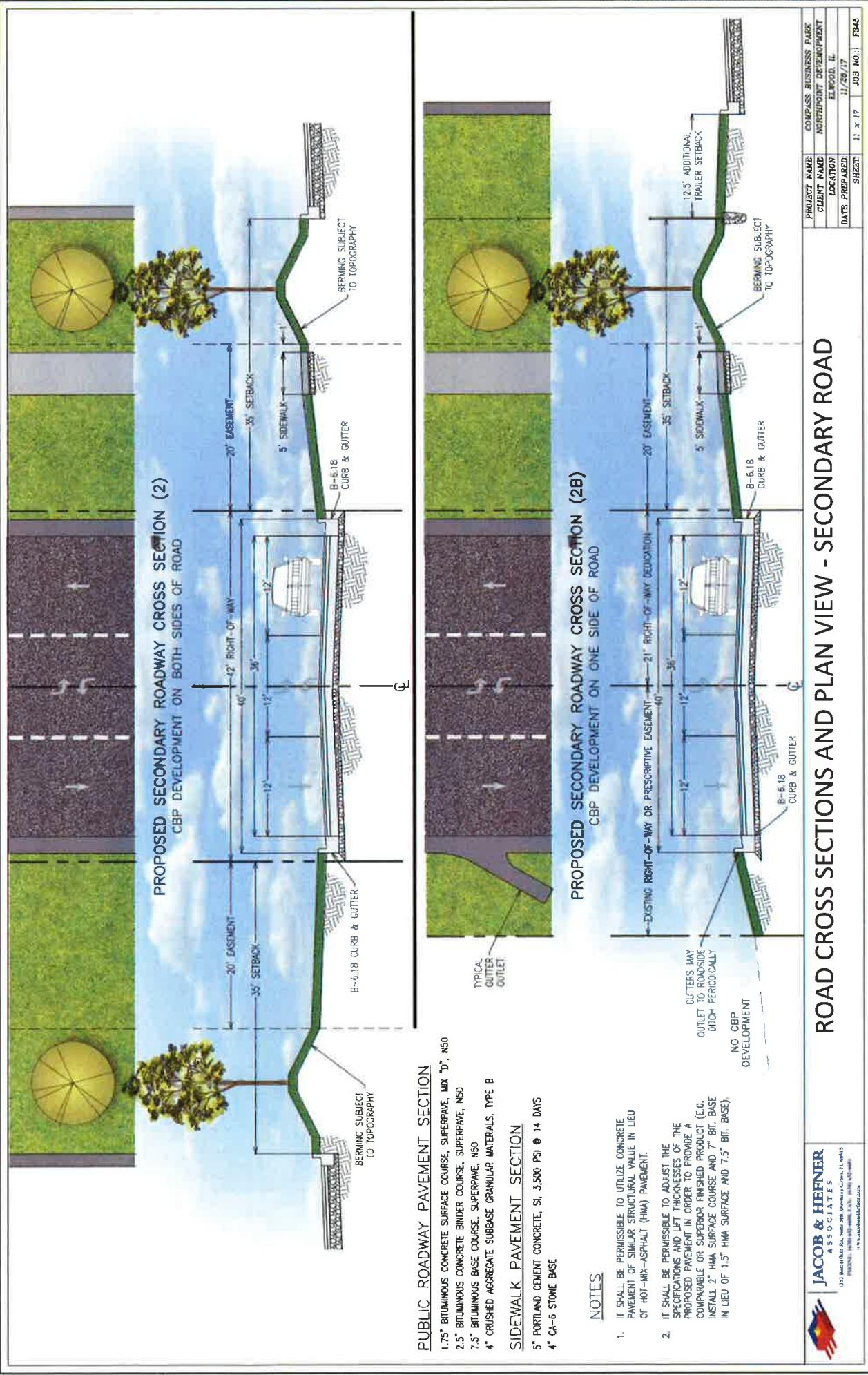
2" BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", NS0
6" AGGREGATE BASE COURSE, TYPE B w/ PRIME COAT

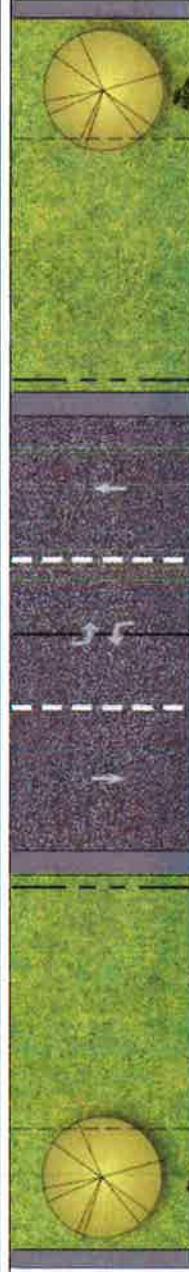
JACOB & HEFFNER
ASSOCIATES
1033 Remington Road, New Lenox, Illinois 60451
PHONE: (708) 422-4460 FAX: (708) 422-4461
e-mail: jacobheffner@jacobheffner.com

ROAD CROSS SECTIONS AND PLAN VIEW - NEW ROAD ON ELWOOD PROPERTY

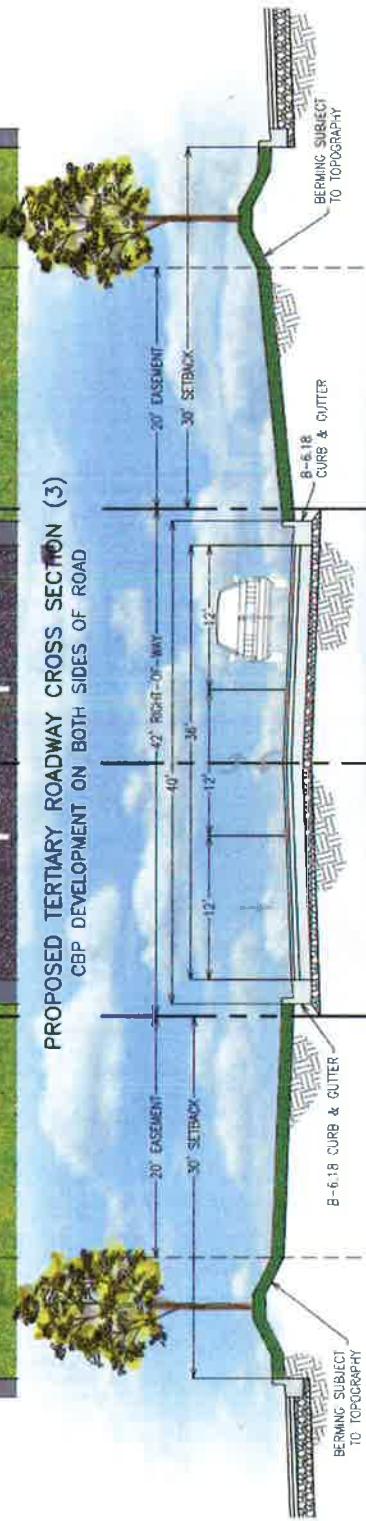
PROJECT NAME:	COLPAS BUSINESS PARK
CLIENT NAME:	NORTHPoint DEVELOPMENT
LOCATION:	ELWOOD, IL
DATE PREPARED:	11/20/17
SHEET	11 x 17 JOB NO.: F3245







PROPOSED TERTIARY ROADWAY CROSS SECTION (3)
CBP DEVELOPMENT ON BOTH SIDES OF ROAD

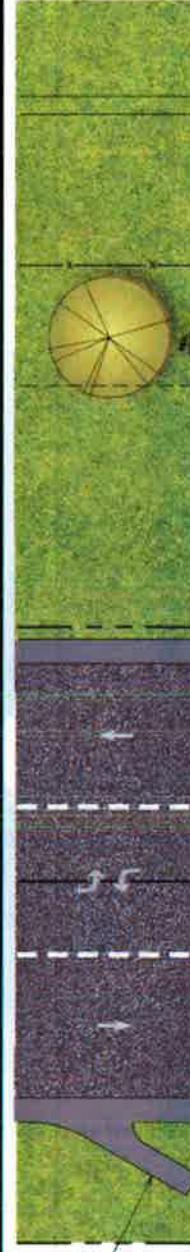


PUBLIC ROADWAY PAVEMENT SECTION

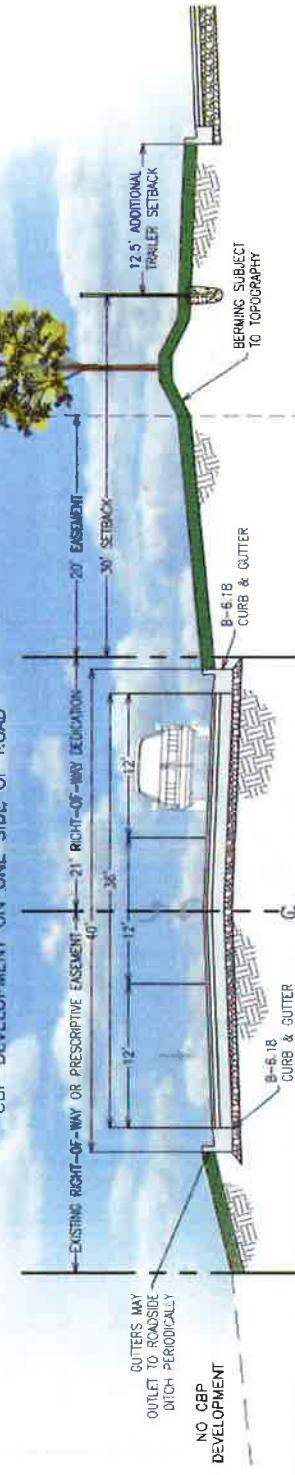
1.75" BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", NS0
2.5" BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, NS0
2.5" BITUMINOUS BASE COURSE, SUPERPAVE, NS0
4" CRUSHED AGGREGATE SUBGRADE GRANULAR MATERIALS, TYPE B

NOTES

1. IT SHALL BE PERMISSIBLE TO UTILIZE CONCRETE PAVEMENT OR SIMILAR STRUCTURAL VALUE IN LIEU OF HOT-MA-ASPHALT (HMA) PAVEMENT.
2. IT SHALL BE PERMISSIBLE TO ADJUST THE SPECIFICATIONS AND LIFT THICKNESSES OF THE PROPOSED PAVEMENT IN ORDER TO PROVIDE A COMPARABLE OR SUPERIOR FINISHED PRODUCT (E.G. INSTALL 2" HMA SURFACE COURSE AND 7' BT. BASE IN LIEU OF 1.5" HMA SURFACE AND 7.5' BT. BASE).



PROPOSED TERTIARY ROADWAY CROSS SECTION (3B)
CBP DEVELOPMENT ON ONE SIDE OF ROAD

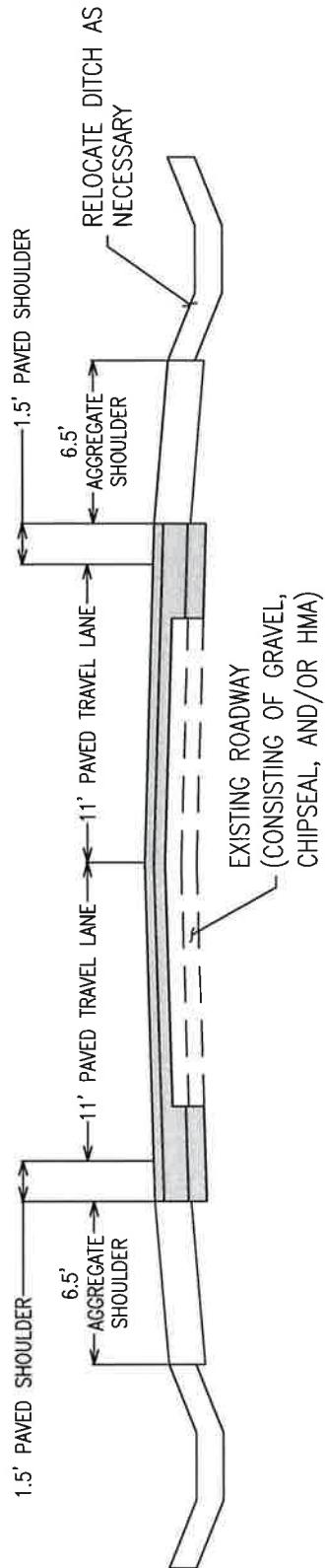


ROAD CROSS SECTIONS AND PLAN VIEW - TERTIARY ROAD

PROJECT NAME:	COMPASS BUSINESS PARK
CLIENT NAME:	NORTHPOINT DEVELOPMENT
LOCATION:	EL WOOD, IL
DATE PREPARED:	11/29/17
HEET:	11 x 17 Job No.: F345

TOWNSHIP ROAD IMPROVEMENTS CROSS SECTION

PROPOSED CROSS SECTION



JACOB & HEFNER
ASSOCIATES
1333 Butterfield Rd, Suite 300, Downers Grove, IL 60545
PHONE: (630) 652-5600, FAX: (630) 652-4601
www.jacobandhefner.com

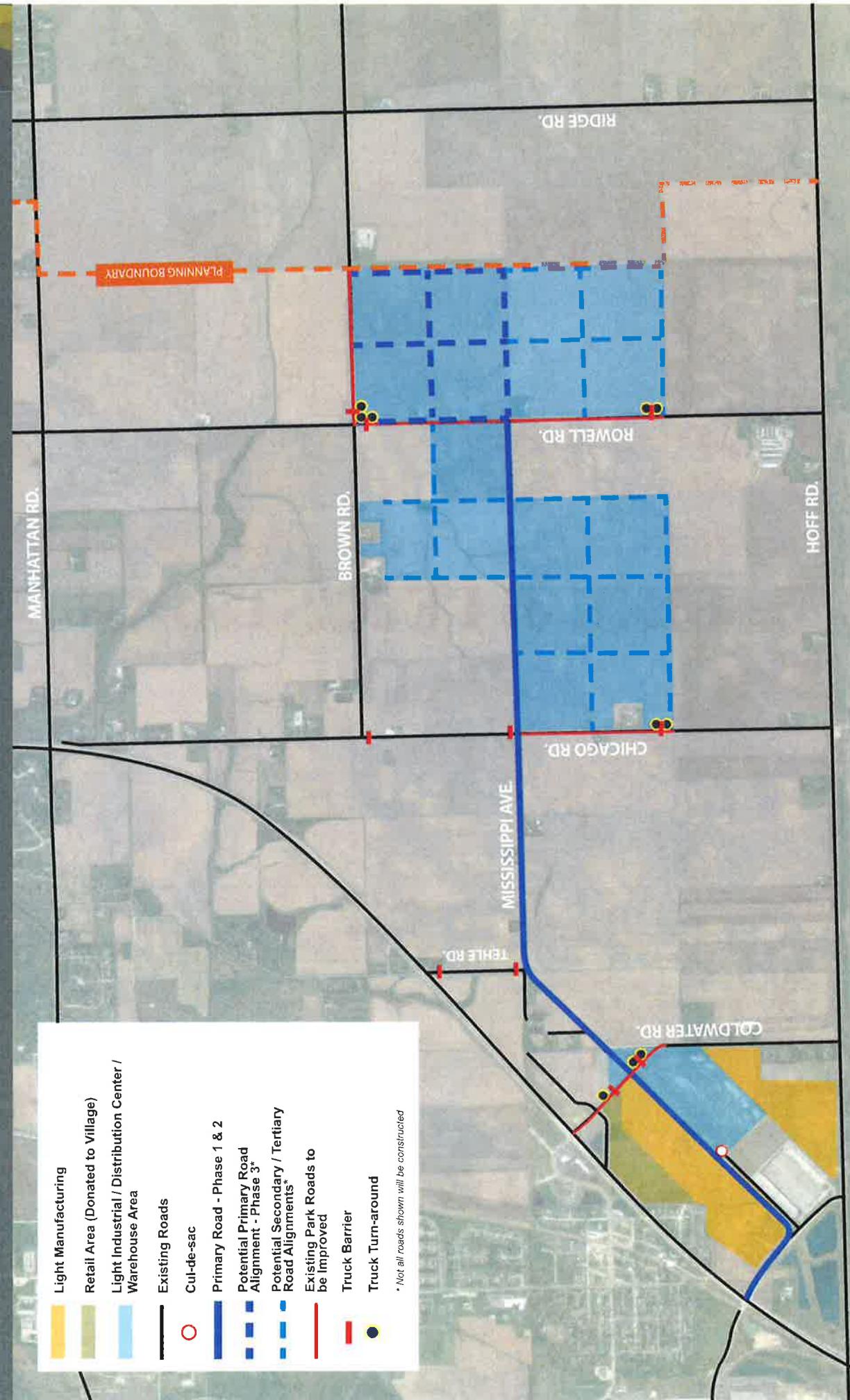


ROAD CROSS SECTIONS AND PLAN VIEW TOWNSHIP ROAD

PROJECT NAME:	COMPASS BUSINESS PARK	
CLIENT NAME:	NORTHPOINT DEVELOPMENT	
LOCATION:	ELWOOD, IL	
DATE PREPARED:	11/29/17	
EXHIBIT	JOB NO.	F345

CONCEPT PLAN

Concept Plan



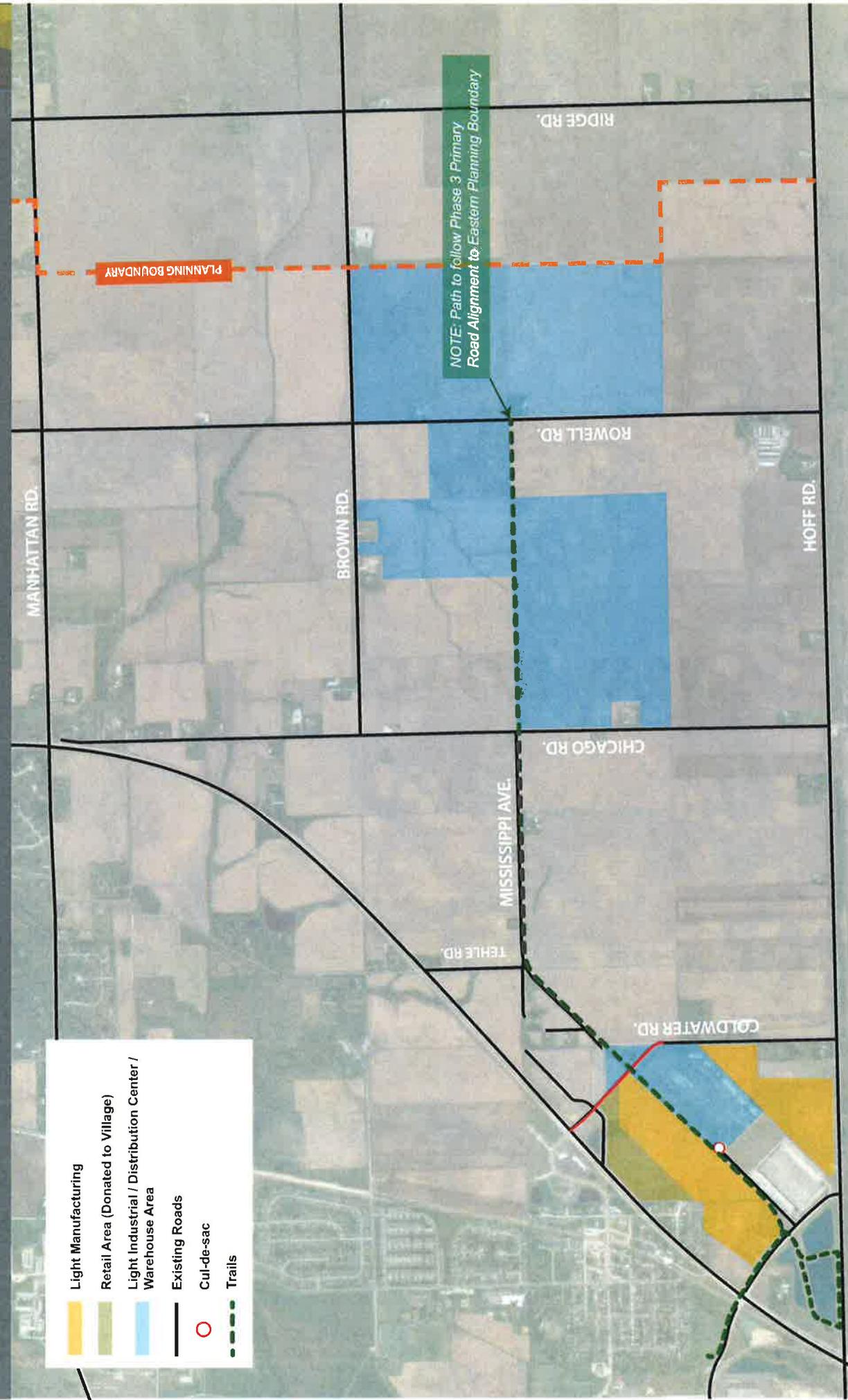
BIKE & PEDESTRIAN PATHS



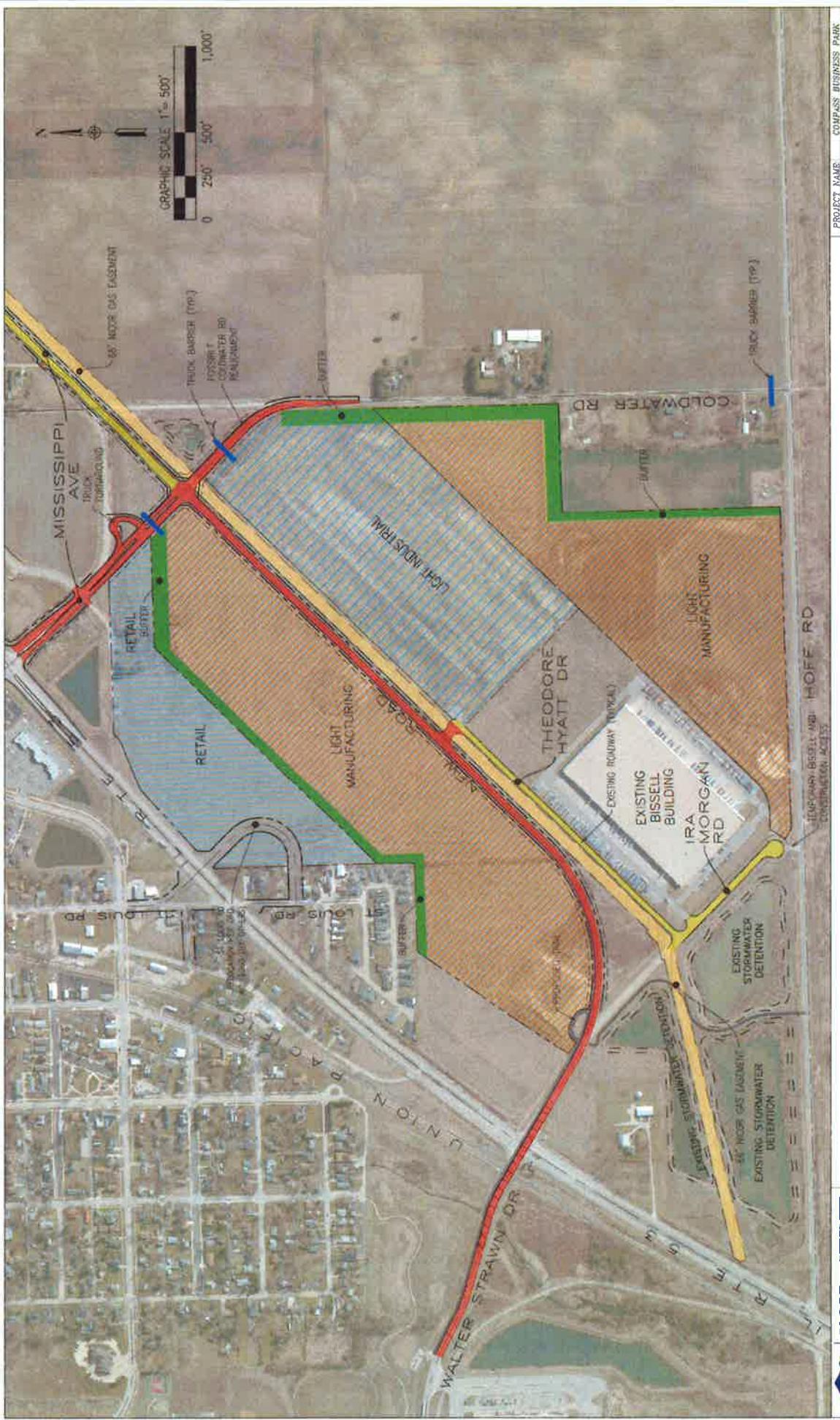
Compass Business Park

Compass
Business Park

Bike and Pedestrian Paths



NEW ROAD LAYOUT



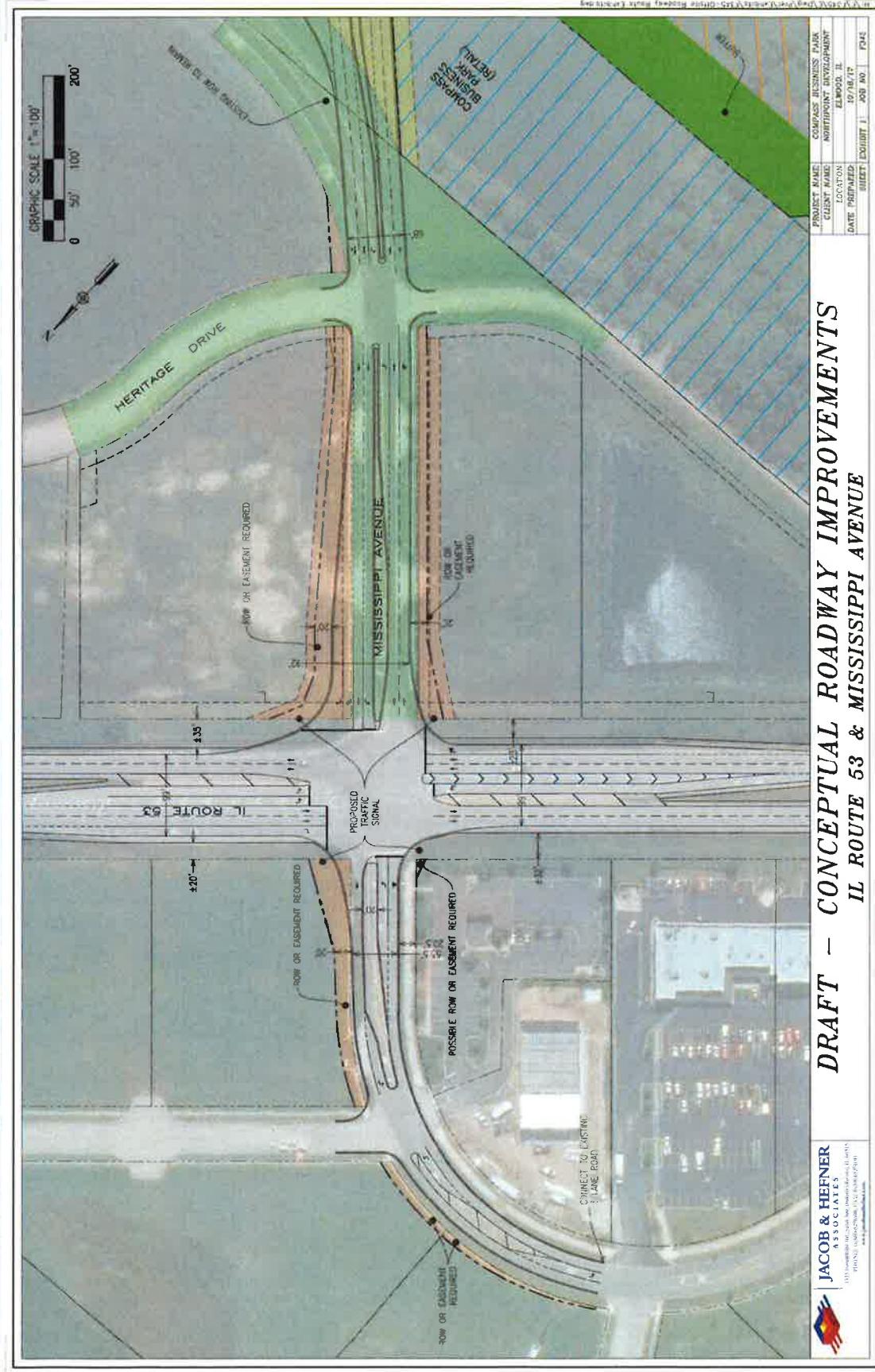
JACOB & HEFFNER
ASSOCIATES

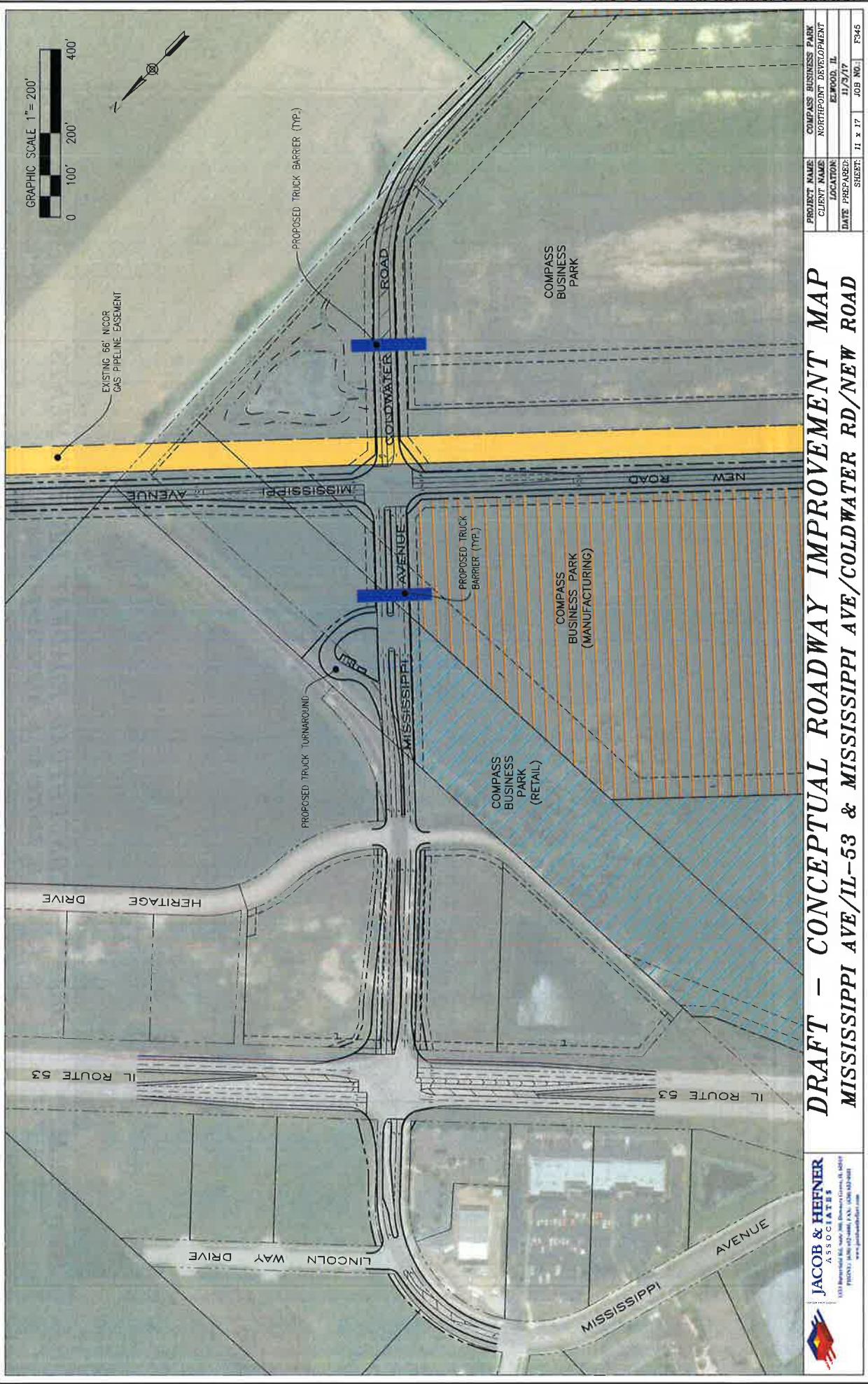
1333 Randolph Street, Suite 200, Chicago, IL 60615
Phone: 312.733.1111 • Fax: 312.733.1115
www.jacobandheffner.com

NEW ROAD

PROJECT NAME	COMPASS BUSINESS PARK
CLIENT NAME	NORTHPOINT DEVELOPMENT
LOCATION	ELWOOD, IL
DATE PREPARED	10/16/17
Sheet:	Job No. 1
Exhibit #:	E245

MISSISSIPPI & ROUTE 53 IMPROVEMENTS

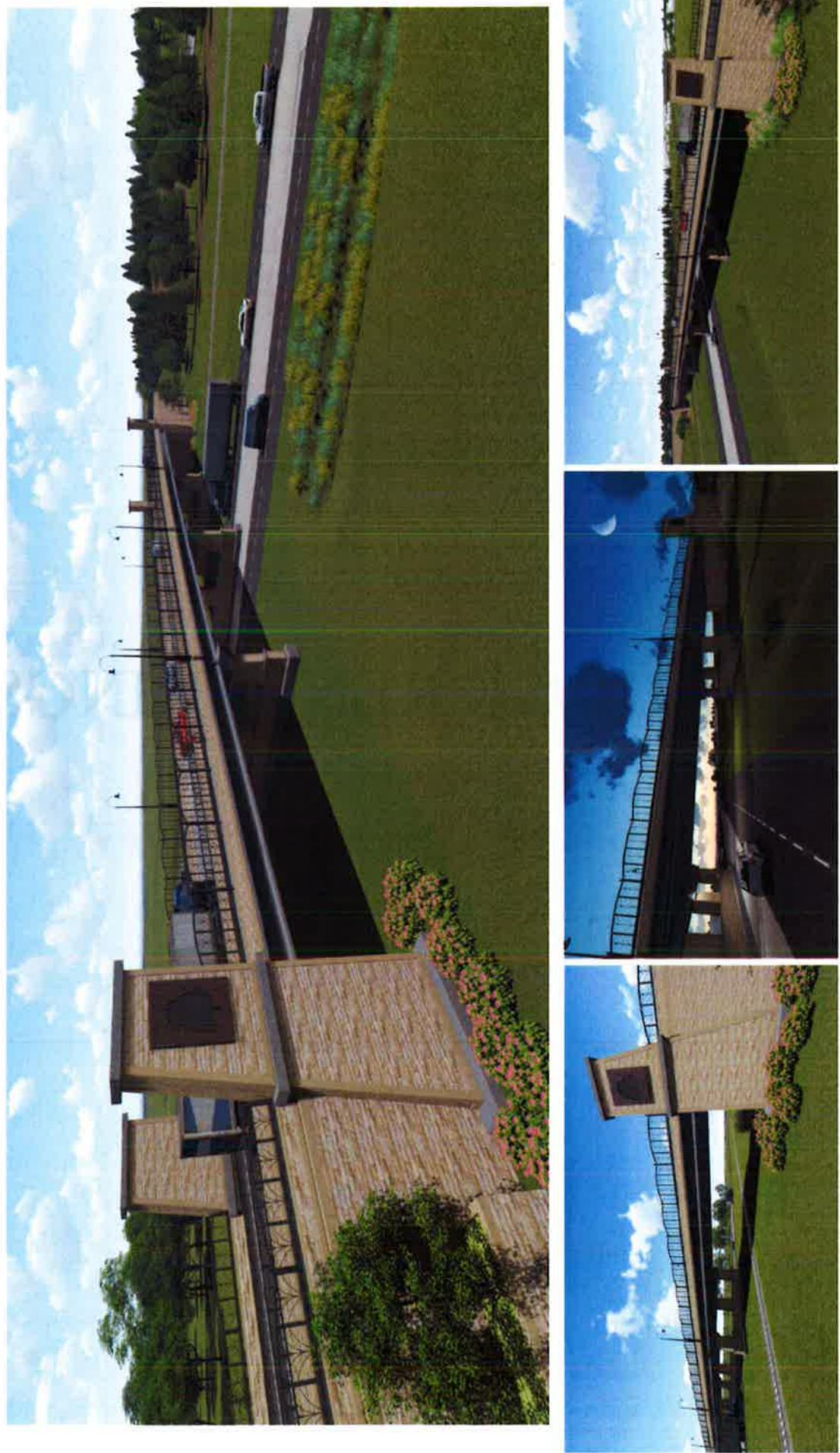




BRIDGE RENDERINGS



Bridge Renderings



BRIDGE RENDERINGS
Page 53 of 78



Bridge Renderings



BRIDGE RENDERINGS



Bridge Renderings



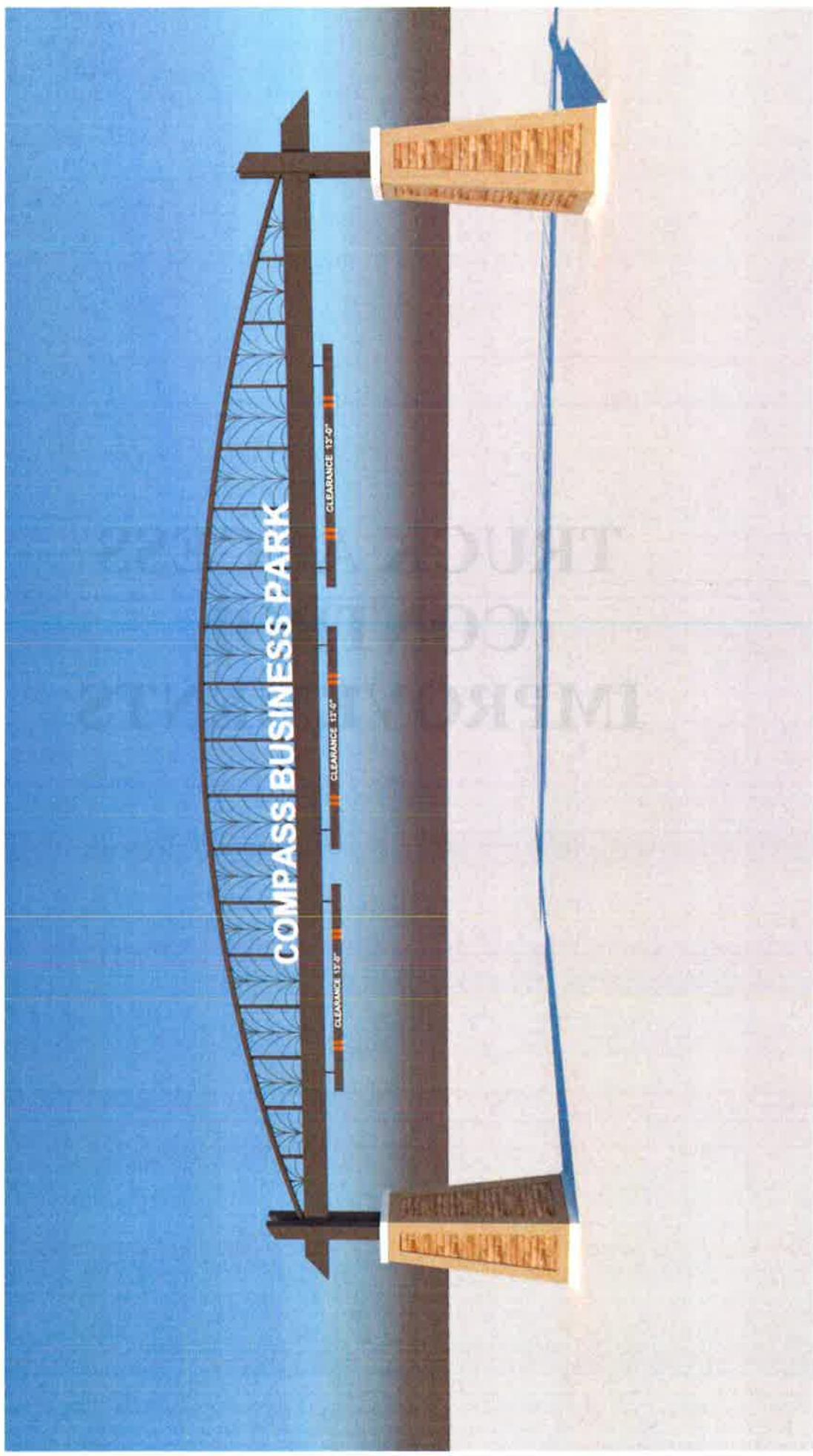
BRIDGE RENDERINGS



Bridge Renderings

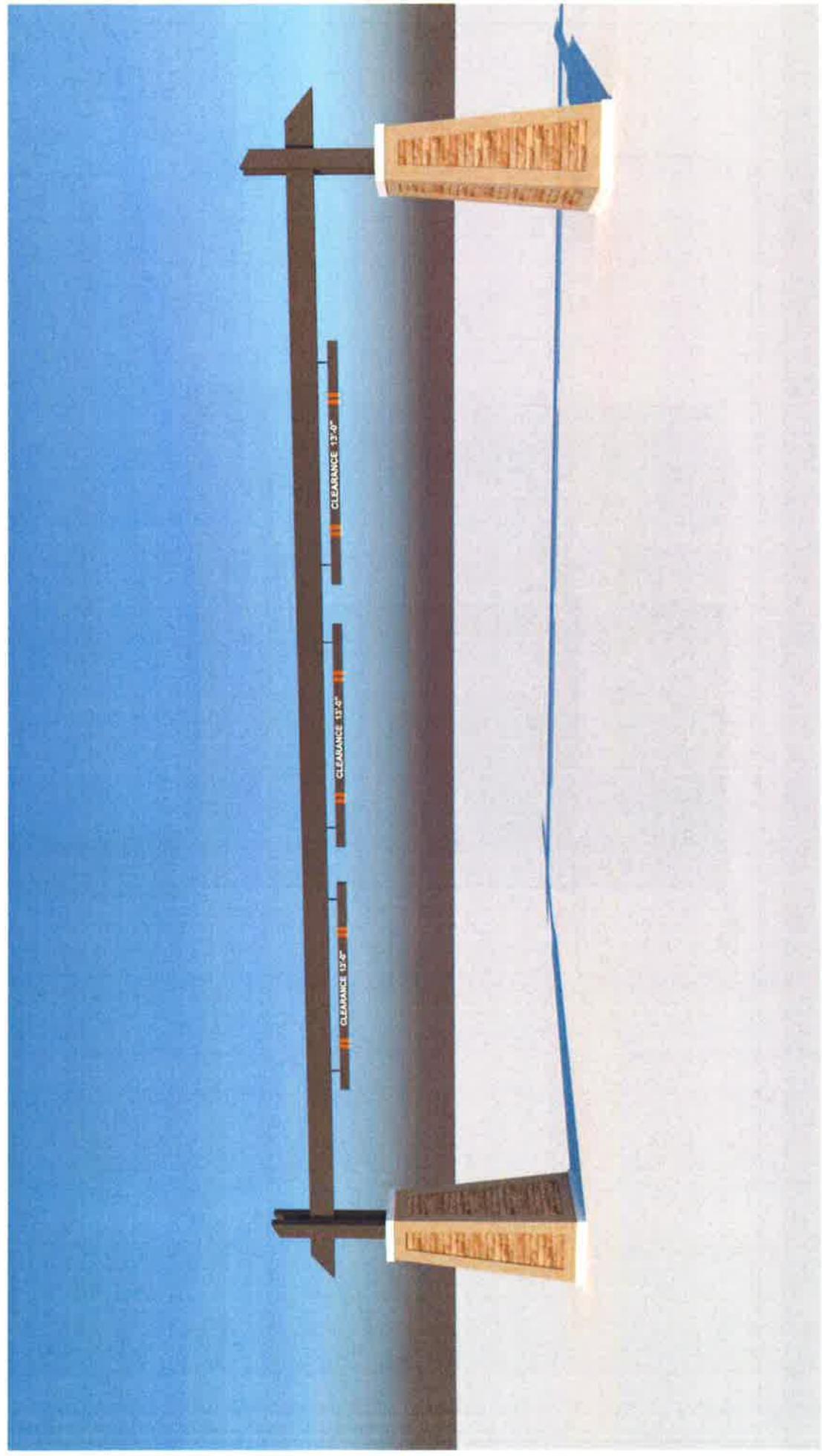
BRIDGE RENDERINGS

TRUCK ACCESS CONTROL IMPROVEMENTS





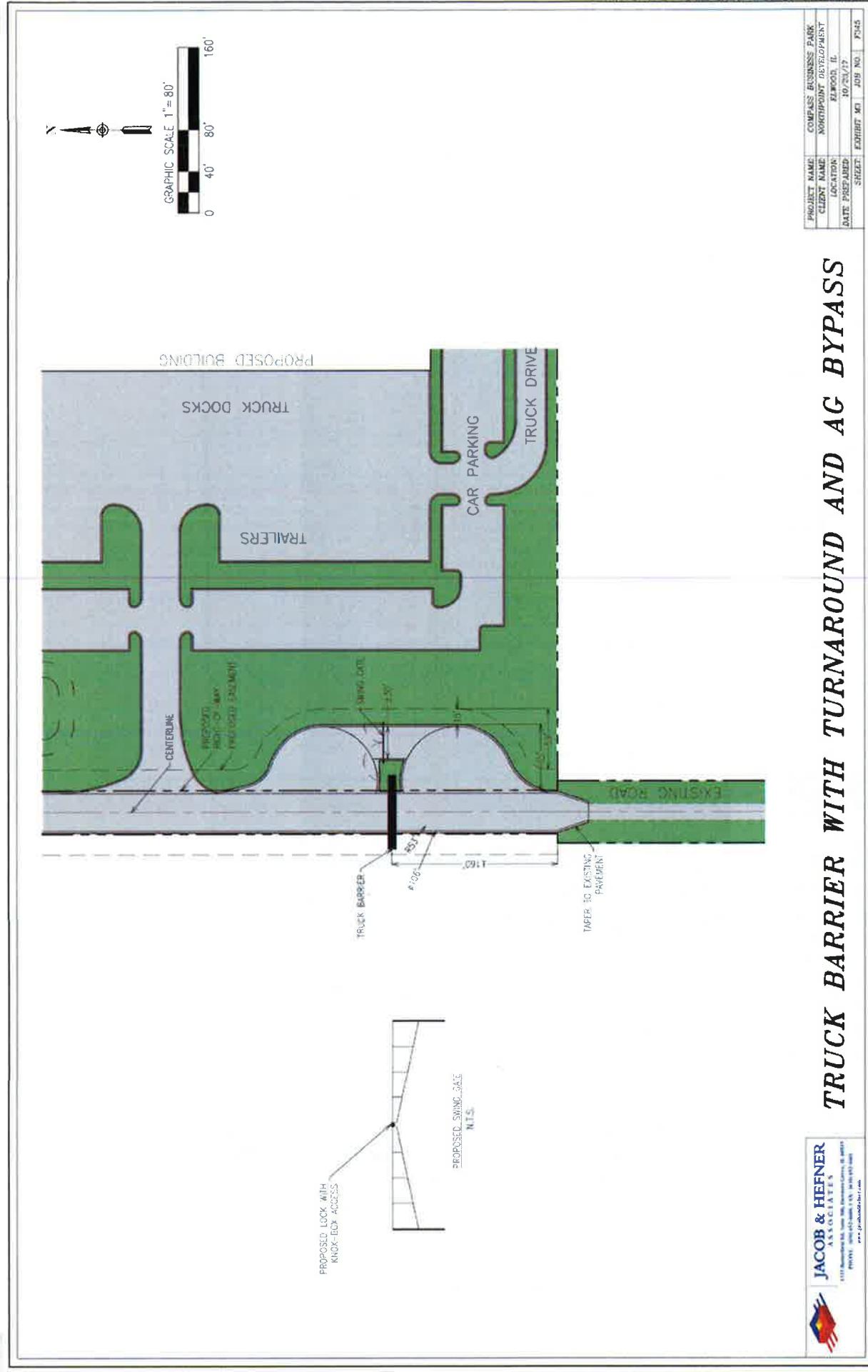
TRUCK ACCESS CONTROL IMPROVEMENTS

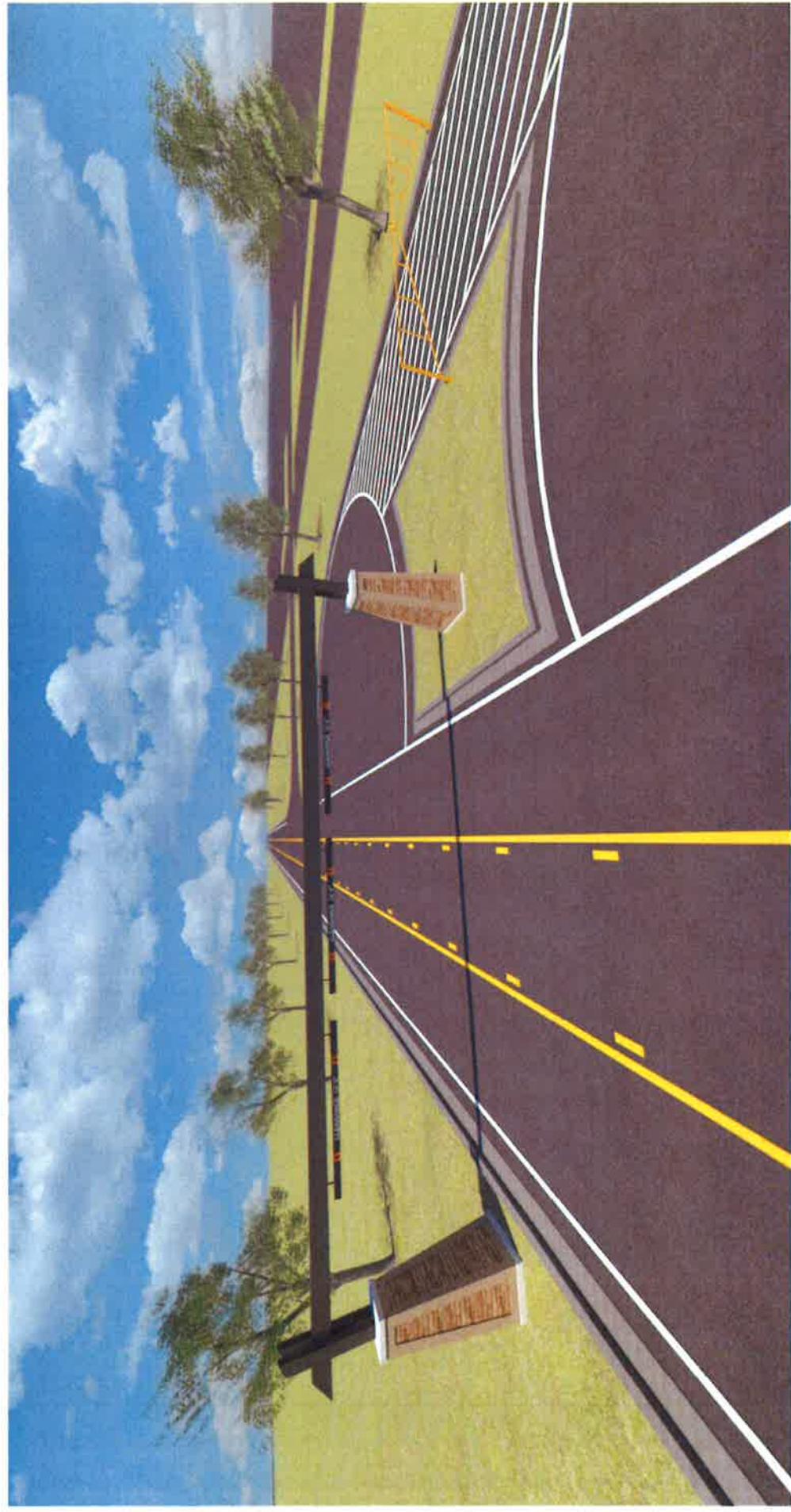


SECONDARY PARK ENTRANCE - TRUCK BARRIER



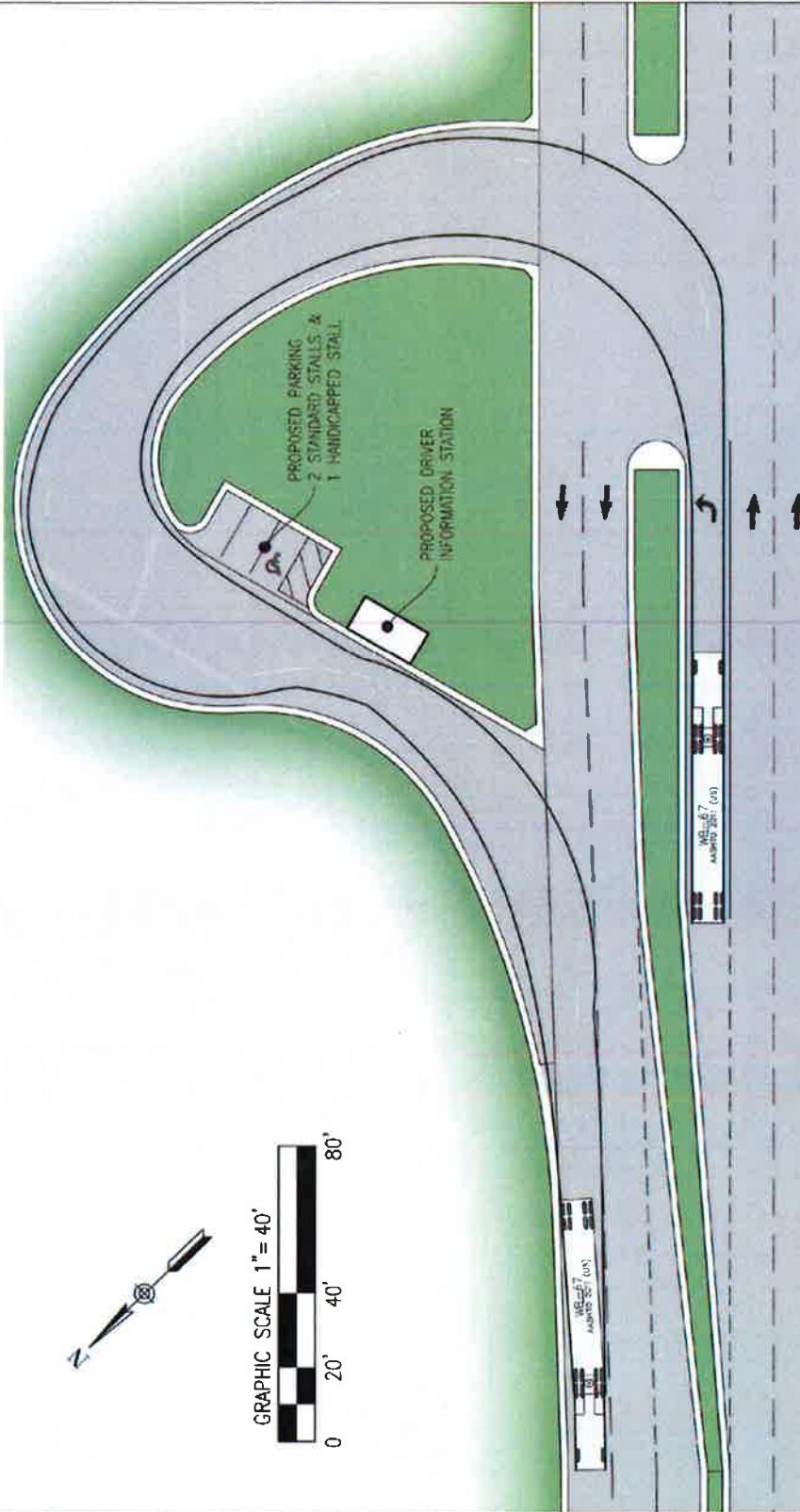
TRUCK ACCESS CONTROL IMPROVEMENTS





TRUCK BARRIER WITH TURNAROUND AND AG BYPASS

MISSISSIPPI ENTRANCE TRUCK TURN-AROUND



H:\V\1\345\1\DRAWING\PROJECTS\X\345-Turning Movements-Mississippi.dwg

PROJECT NAME:	COMPASS BUSINESS PARK		
CLIENT NAME:	NORTHPoint DEVELOPMENT		
LOCATION:	ELWOOD, IL		
DATE PREPARED:	10/26/17		
SHEET:	8.5 x 11	JOB NO.	P345



JACOB & HEFNER
ASSOCIATES

1335 Butterfield Rd, Suite 300, Downers Grove, IL 60115
PHONE: (708) 925-4481 FAX: (708) 925-4481
www.jacobandhefner.com

ADDITIONAL EXEMPLARY FEATURES OF COMPASS BUSINESS PARK



Additional Exemplary Features of Compass Business Park



Slide 1: Building with Landscaping



Additional Exemplary Features of Compass Business Park



Slide 2: Pedestrian Access to Midwin



Additional Exemplary Features of Compass Business Park



Slide 3: Pedestrian Path over Bridge



Additional Exemplary Features of Compass Business Park



Slide 4: Bridge View from Southeast



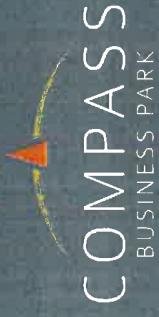
Slide 5: Mississippi and Route 53 Improvements – Parkway Entry



Additional Exemplary Features of Compass Business Park



Slide 6: Bridge View at Dusk



Additional Exemplary Features of Compass Business Park



Slide 7: Mississippi Realignment Parkway Entry with Overhead Barrier



Additional Exemplary Features of Compass Business Park



Slide 8: Bridge View from Northeast
Page 77 of 115



Additional Exemplary Features of Compass Business Park



Slide 9: Bridge View from Northwest at Ground Level
Page 78 of 79



Additional Exemplary Features of Compass Business Park



Slide 10: Mississippi Road Parkway Entry with Turnaround

OWNER'S AFFIDAVIT

The undersigned, being duly sworn on oath, does state that he/she is the owner or legal representative of the owner of the property as set forth herein and that NorthPoint Development, LLC and/or East Gate – Logistics Park Chicago, LLC have been authorized to submit an application to the Village of Elwood, Illinois for all rezoning, planned unit developments, preliminary and/or final plats, concept plans and similar requests and entitlements upon the property located at:

TRACT "E" (P.I.N. 10-11-29-207-004-0000)

ALL OF LOT 2 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS

and that the employees, agents and elected and appointed officials of the Village of Elwood, Illinois hereby are granted permission to enter onto the property to perform a visual inspection and familiarize themselves with conditions of the property during reasonable hours. This permission is granted in connection with the above-named applications pending before the Village of Elwood, Illinois.

[Separate Signature Page Follows]

OWNER:

COMPASS BUSINESS PARK OWNER'S
ASSOCIATION f/k/a CenterPoint Intermodal
Center-East Property Owners' Association

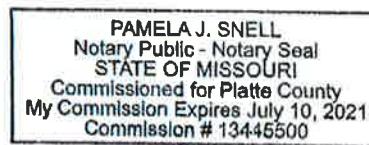
By:

Nathaniel Hagedorn, its President

STATE OF MISSOURI)
) SS.
COUNTY OF PLATTE)

Subscribed and sworn to before me this 30th day of November, 2017.

Notary Public



OWNER'S CONSENT

The undersigned, being duly sworn on oath, does state that he/she is the owner or legal representative of the owner of the property as set forth herein and that NorthPoint Development, LLC and/or East Gate – Logistics Park Chicago, LLC have been authorized to submit an application to the Village of Elwood, Illinois for all rezoning, planned unit developments, preliminary and/or final plats, concept plans and similar requests and entitlements upon the property located at:

TRACT "M" (P.I.N. 10-11-22-400-010-0000)

THE SOUTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS. ALSO, THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, EXCEPTING THEREFROM THE NORTH 400 FEET OF THE WEST 415 FEET THEREOF AND ALSO EXCEPTING THAT PART OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS COMMENCING AT THE NORTHEAST CORNER OF SAID SOUTHEAST QUARTER; THENCE WEST, ALONG THE NORTH LINE OF SAID SOUTHEAST QUARTER 1664.12 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING WEST, ALONG SAID NORTH LINE 300.00 FEET; THENCE SOUTH, PERPENDICULAR TO THE LAST DESCRIBED COURSE, 363.00 FEET; THENCE EAST, PERPENDICULAR TO THE LAST DESCRIBED COURSE, 300.00 FEET; THENCE NORTH, PERPENDICULAR TO THE LAST DESCRIBED COURSE, 363.00 FEET, TO THE POINT OF BEGINNING, IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

TRACT "N" (P.I.N. 10-11-22-400-007-0000)

THE NORTH 10 ACRES OF THE SOUTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

TRACT "O" (P.I.N. 10-11-22-400-008-0000)

THE SOUTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22 IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, EXCEPT THE NORTH 10 ACRES THEREOF, IN WILL COUNTY, ILLINOIS.

and that the employees, agents and elected and appointed officials of the Village of Elwood, Illinois hereby are granted permission to enter onto the property to perform a visual inspection and familiarize themselves with conditions of the property during reasonable hours. This permission is granted in connection with the above-named applications pending before the Village of Elwood, Illinois.

[Separate Signature Page Follows]

OWNER:

EGLPC Brown Property, LLC, a Missouri limited liability company

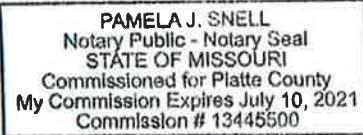
By: NorthPoint Development, LLC, its
Manager

By: Nathaniel Hagedorn, its Manager

STATE OF MISSOURI)
) SS.
COUNTY OF PLATTE)

Subscribed and sworn to before me this 30 day of October, 2017.

Notary Public



OWNER'S AFFIDAVIT

The undersigned, being duly sworn on oath, does state that he/she is the owner or legal representative of the owner of the property as set forth herein and that NorthPoint Development, LLC and/or East Gate – Logistics Park Chicago, LLC have been authorized to submit an application to the Village of Elwood, Illinois for all rezoning, planned unit developments, preliminary and/or final plats, concept plans and similar requests and entitlements upon the property located at:

TRACT "B" (P.I.N. 10-11-29-401-003-0000)

ALL OF LOT 4 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS.

TRACT "D" (P.I.N. 10-11-29-401-001-0000)

ALL OF LOT 5 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS.

TRACT "E" (P.I.N. 10-11-29-207-004-0000)

ALL OF LOT 2 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS.

and that the employees, agents and elected and appointed officials of the Village of Elwood, Illinois hereby are granted permission to enter onto the property to perform a visual inspection and familiarize themselves with conditions of the property during reasonable hours. This permission is granted in connection with the above-named applications pending before the Village of Elwood, Illinois.

[Separate Signature Page Follows]

OWNER:

LB Andersen Land Holding, LLC, an Illinois limited liability company

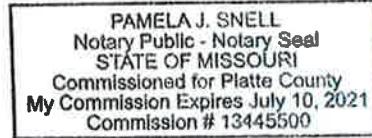
By: East Gate – Logistics Park Chicago, its Member

By: Nathaniel Hagedorn, its Manager

STATE OF MISSOURI)
) SS.
COUNTY OF PLATTE)

Subscribed and sworn to before me this 31st day of October, 2017.

Notary Public



OWNER'S CONSENT

The undersigned, being duly sworn on oath, does state that he/she is the owner or legal representative of the owner of the property as set forth herein and that NorthPoint Development, LLC and/or East Gate – Logistics Park Chicago, LLC have been authorized to submit an application to the Village of Elwood, Illinois for all rezoning, planned unit developments, preliminary and/or final plats, concept plans and similar requests and entitlements upon the property located at:

TRACT "I" (P.I.N. 10-11-27-100-008-0000)

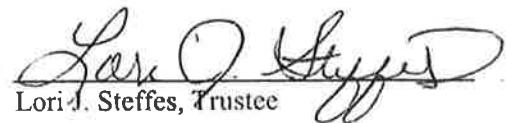
THE WEST 1347.98 FEET OF THE NORTHWEST QUARTER, EXCEPT THE SOUTH 1343.14 FEET THEREOF, IN SECTION 27, IN TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

and that the employees, agents and elected and appointed officials of the Village of Elwood, Illinois hereby are granted permission to enter onto the property to perform a visual inspection and familiarize themselves with conditions of the property during reasonable hours. This permission is granted in connection with the above-named applications pending before the Village of Elwood, Illinois.

[Separate Signature Page Follows]

OWNER:

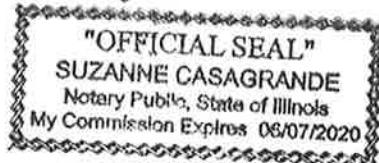
Lori J. Steffes, as Trustee of the Lori J.
Steffes Trust dated November 18, 2013

By: 
Lori J. Steffes, Trustee

STATE OF ILLINOIS)
COUNTY OF Will) SS.

Subscribed and sworn to before me this 15th day of September, 2017.

Suzanne Casagrande
Notary Public



OWNER'S CONSENT

The undersigned, being duly sworn on oath, does state that he/she is the owner or legal representative of the owner of the property as set forth herein and that NorthPoint Development, LLC and/or East Gate – Logistics Park Chicago, LLC have been authorized to submit an application to the Village of Elwood, Illinois for all rezoning, planned unit developments, preliminary and/or final plats, concept plans and similar requests and entitlements upon the property located at:

TRACT "K" (P.I.N. 10-11-27-100-005-0000)

THE SOUTHERN 1343.14 FEET OF THE WEST HALF OF THE NORTHEAST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS. ALSO, THE SOUTHERN 1343.14 FEET OF THE NORTHWEST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS, EXCEPTING THEREFROM THE NORTH 466.70 FEET OF THE SOUTH 996.70 FEET OF THE WEST 466.70 FEET THEREOF.

TRACT "L" (P.I.N. 10-11-27-100-007-0000)

THE WEST HALF OF THE NORTHEAST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS, EXCEPT THE SOUTHERN 1343.14 FEET THEREOF. ALSO, THE NORTHWEST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS, EXCEPT THE SOUTHERN 1343.14 FEET THEREOF AND ALSO EXCEPTING THE WESTERN 1347.98 FEET THEREOF.

and that the employees, agents and elected and appointed officials of the Village of Elwood, Illinois hereby are granted permission to enter onto the property to perform a visual inspection and familiarize themselves with conditions of the property during reasonable hours. This permission is granted in connection with the above-named applications pending before the Village of Elwood, Illinois.

[Separate Signature Page Follows]

OWNER:

Robert E. Steffes Family Partnership LP

By:



Rex C. Steffes, its General Partner

By:



Robert E. Steffes, Trustee

STATE OF ILLINOIS)

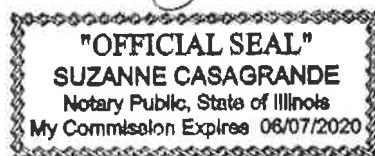
) SS.

COUNTY OF Will)

Subscribed and sworn to before me this 15th day of September, 2017.



Notary Public



OWNER'S CONSENT

The undersigned, being duly sworn on oath, does state that he/she is the owner or legal representative of the owner of the property as set forth herein and that NorthPoint Development, LLC and/or East Gate – Logistics Park Chicago, LLC have been authorized to submit an application to the Village of Elwood, Illinois for all rezoning, planned unit developments, preliminary and/or final plats, concept plans and similar requests and entitlements upon the property located at:

TRACT "Q" (P.I.N. 10-11-26-100-004-0000)

**THE WEST 660 FEET OF THE NORTH 660 FEET OF THE NORTHWEST QUARTER
OF SECTION 26, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD
PRINCIPAL MERIDIAN, IN JACKSON TOWNSHIP, IN WILL COUNTY, ILLINOIS.**

and that the employees, agents and elected and appointed officials of the Village of Elwood, Illinois hereby are granted permission to enter onto the property to perform a visual inspection and familiarize themselves with conditions of the property during reasonable hours. This permission is granted in connection with the above-named applications pending before the Village of Elwood, Illinois.

[Separate Signature Page Follows]

OWNER:


Donald Walden, Jr.

STATE OF ILLINOIS)
) SS.
COUNTY OF WILL)

Subscribed and sworn to before me this 13 day of October, 2017.

Joh. S. Gell
Notary Public



OWNER'S CONSENT

The undersigned, being duly sworn on oath, does state that he/she is the owner or legal representative of the owner of the property as set forth herein and that NorthPoint Development, LLC and/or East Gate – Logistics Park Chicago, LLC have been authorized to submit an application to the Village of Elwood, Illinois for all rezoning, planned unit developments, preliminary and/or final plats, concept plans and similar requests and entitlements upon the property located at:

TRACT "R" (P.I.N. 10-11-26-100-003-0000)

THE NORTH HALF OF THE NORTHWEST QUARTER OF SECTION 26, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, EXCEPTING THEREFROM THE WEST 660 FEET OF THE NORTH 660 FEET OF THE NORTHWEST QUARTER OF SECTION 26, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, ALL IN WILL COUNTY, ILLINOIS.

TRACT "S" (P.I.N. 10-11-26-100-002-0000)

THE SOUTH HALF OF THE NORTHWEST QUARTER OF SECTION 26, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

and that the employees, agents and elected and appointed officials of the Village of Elwood, Illinois hereby are granted permission to enter onto the property to perform a visual inspection and familiarize themselves with conditions of the property during reasonable hours. This permission is granted in connection with the above-named applications pending before the Village of Elwood, Illinois.

[Separate Signature Page Follows]

OWNER:

Estate of Donald D. Walden, Sr., Deceased.

By:

Name: David A. Marchio

Its: Independent Executor

STATE OF ILLINOIS)
) SS.
COUNTY OF WILL)

Subscribed and sworn to before me this 13 day of October, 2017.

John S. Gable
Notary Public



OWNER'S CONSENT

The undersigned, being duly sworn on oath, does state that he/she is the owner or legal representative of the owner of the property as set forth herein and that NorthPoint Development, LLC and/or East Gate – Logistics Park Chicago, LLC have been authorized to submit an application to the Village of Elwood, Illinois for all rezoning, planned unit developments, preliminary and/or final plats, concept plans and similar requests and entitlements upon the property located at:

TRACT "P" (P.I.N. 10-11-23-300-001-0000)
THE SOUTHWEST QUARTER OF SECTION 23, TOWNSHIP 34 NORTH, RANGE 10
EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

and that the employees, agents and elected and appointed officials of the Village of Elwood, Illinois hereby are granted permission to enter onto the property to perform a visual inspection and familiarize themselves with conditions of the property during reasonable hours. This permission is granted in connection with the above-named applications pending before the Village of Elwood, Illinois.

[Separate Signature Page Follows]

OWNER:

Riegel
Bernard F. Riegel, Jr. *BR*

Bernard F. Riegel Jr.

STATE OF ILLINOIS)
) SS.
COUNTY OF *Grundy*)

Subscribed and sworn to before me this 11 day of October, 2017.

Jessica Renee Shultz
Notary Public



OWNER:

Jeanne A. Williams, as Trustee of the Jeanne A. Williams Living Trust dated October 12, 2015

By: Jeanne A. Williams
Jeanne A. Williams, Trustee

Williams-Riegel Property, LLC

By: Jeanne A. Williams
Jeanne A. Williams, Member

Missouri
STATE OF ILLINOIS)
) SS.
COUNTY OF Caldwell)

Subscribed and sworn to before me this 14th day of September, 2017.

Amanda McGinley
Notary Public



**REZONING DEPICTION
AND
LEGAL DESCRIPTION**

Rezoning Legal Description

TRACT "B" (P.I.N. 10-11-29-401-003-0000)

ALL OF LOT 4 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS.

TRACT "D" (P.I.N. 10-11-29-401-001-0000)

ALL OF LOT 5 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS.

TRACT "E" (P.I.N. 10-11-29-207-004-0000)

ALL OF LOT 2 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS.

TRACT "F" (P.I.N. 10-11-29-207-003-0000)

ALL OF LOT 6 IN CENTERPOINT INTERMODAL CENTER EAST UNIT ONE, BEING A SUBDIVISION OF PART OF SECTION 29, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED DECEMBER 22, 2008, AS DOCUMENT NUMBER 2008-147917, IN WILL COUNTY, ILLINOIS.

TRACT "I" (P.I.N. 10-11-27-100-008-0000)

THE WEST 1347.98 FEET OF THE NORTHWEST QUARTER, EXCEPT THE SOUTH 1343.14 FEET THEREOF, IN SECTION 27, IN TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

TRACT "K" (P.I.N. 10-11-27-100-005-0000)

THE SOUTHERN 1343.14 FEET OF THE WEST HALF OF THE NORTHEAST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS. ALSO, THE SOUTHERN 1343.14 FEET OF THE NORTHWEST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS, EXCEPTING THEREFROM THE NORTH 466.70 FEET OF THE SOUTH 996.70 FEET OF THE WEST 466.70 FEET THEREOF.

TRACT "L" (P.I.N. 10-11-27-100-007-0000)

THE WEST HALF OF THE NORTHEAST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS, EXCEPT THE SOUTHERN 1343.14 FEET THEREOF. ALSO, THE NORTHWEST QUARTER OF SECTION 27, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS, EXCEPT THE SOUTHERN 1343.14 FEET THEREOF AND ALSO EXCEPTING THE WESTERN 1347.98 FEET THEREOF.

TRACT "M" (P.I.N. 10-11-22-400-010-0000)

THE SOUTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS. ALSO, THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, EXCEPTING THEREFROM THE NORTH 400 FEET OF THE WEST 415 FEET THEREOF AND ALSO EXCEPTING THAT PART OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, TOWNSHIP 34 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS COMMENCING AT THE NORTHEAST CORNER OF SAID SOUTHEAST QUARTER; THENCE WEST, ALONG THE NORTH LINE OF SAID SOUTHEAST QUARTER 1664.12 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING WEST, ALONG SAID NORTH LINE 300.00 FEET; THENCE SOUTH, PERPENDICULAR TO THE LAST DESCRIBED COURSE, 363.00 FEET; THENCE EAST, PERPENDICULAR TO THE LAST DESCRIBED COURSE, 300.00 FEET; THENCE NORTH, PERPENDICULAR TO THE LAST DESCRIBED COURSE, 363.00 FEET, TO THE POINT OF BEGINNING, IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

TRACT "N" (P.I.N. 10-11-22-400-007-0000)

THE NORTH 10 ACRES OF THE SOUTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22, IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.

TRACT "O" (P.I.N. 10-11-22-400-008-0000)

THE SOUTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 22 IN TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, EXCEPT THE NORTH 10 ACRES THEREOF, IN WILL COUNTY, ILLINOIS.

TRACT "P" (P.I.N. 10-11-23-300-001-0000)

THE SOUTHWEST QUARTER OF SECTION 23, TOWNSHIP 34 NORTH, RANGE 10 EAST, OF THE THIRD PRINCIPAL MERIDIAN, IN WILL COUNTY, ILLINOIS.



Phase I Rezoning - Parcel Proximity List

Property: **Compass Business Park**
Elwood, Illinois

JHA Project #: F345

Date: 9/6/2017

Entity/Owner	PIN	Address	Northpoint Notes	JHA Notes
HARTZ CONSTRUCTION TR HBT 1939	1011213000010000	9026 HERITAGE PKWY WOODRIDGE IL 60517 26737 S		
LARRY L. COLDWATER	1011213020030000	COLDWATER RD ELWOOD IL 60421		
LOUIS S. RODGERS AND JUDY L. RODGERS AS JOINT TENANTS	1011213020040000	19660 MISSISSIPPI RD ELWOOD IL 60421		
BERNHARD FARMS, INC.	1011214000090000	18530 W HOFF RD ELWOOD IL 60421		
BERNHARD FARMS, INC	1011214000100000	18530 W HOFF RD ELWOOD IL 60421		
ROBERT C. HAUERT AND ANGEL HAUERT, HUSBAND AND WIFE AS TENANTS BY THE ENTIRETY	1011214000120000	19510 MISSISSIPPI AVE ELWOOD IL 60421		
COLDWATER LARRY KATHLEEN	1011214000130000	26845 COLDWATER RD ELWOOD IL 60421		
LARRY COLDWATER AND JULIA BAUM-COLDWATER AS JOINT TENANTS	1011214000140000	26737 S COLDWATER RD ELWOOD IL 60421		
TYLER THOMAS A TRUST TYLER CAROL V TRUST	1011222000060000	2715 MARYLAND AVE FLOSSMOOR IL 60422		
TYLER THOMAS A TRUST TYLER CAROL V TRUST	1011222000070000	2715 MARYLAND AVE FLOSSMOOR IL 60422		
TYLER THOMAS A TRUST TYLER CAROL V TRUST	1011222000080000	2715 MARYLAND AVE FLOSSMOOR IL 60422		
TYLER THOMAS A TRUST TYLER CAROL V TRUST	1011222000090000	2715 MARYLAND AVE FLOSSMOOR IL 60422		
TYLER THOMAS A TRUST TYLER CAROL V TRUST	1011222000100000	2715 MARYLAND AVE FLOSSMOOR IL 60422		
TYLER THOMAS A TRUST TYLER CAROL V TRUST	1011222000130000	2715 MARYLAND AVE FLOSSMOOR IL 60422		
VAB FARMS LLC	1011223000050000	20 CARLYLE LN BUFFALO GROVE IL 60089		
BERNHARD FARMS, INC.	1011223000120000	18530 W HOFF RD ELWOOD IL 60421		

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Phase I Rezoning - Parcel Proximity List

Property: **Compass Business Park**
Elwood, Illinois

JHA Project #: F345

Date: 9/6/2017

Entity/Owner	PIN	Address	Northpoint Notes	JHA Notes
CROY WAYNE CARLA REV		108 W GARDNER ST		
FMLY TR	1011224000040000	PO BOX 104		
ARENDE JEANNINE TRUST		ELWOOD IL 60421		
WALSH LAWRENCE M		18801 W BROWN		
IRENE		RD		
SIEVERT MICHAEL L TRUST	1011224000110000	ELWOOD IL 60421		
ARNHOLD WALTER J DEC		24212 S ROWELL		
TRUST	1011231000030000	RD		
ARNHOLD CYNTHIA L DEC		ELWOOD IL 60421		
TRUST				
CHICAGO TITLE LAND		10 S LASALLE ST		
TRUST CO	1011232000020000	STE 2750		
TR 8002367519		CHICAGO IL 60603		
DART INC TR 183		26057 S RIDGE RD		
C/O ROBERT C SWEEDLER	1011234000040000	ELWOOD IL 60421		
DART INC C/O ROBERT		26057 S RIDGE RD		
SWEEDLER	1011234000050000	ELWOOD IL 60421		
BERNHARD FARMS INC	1011262000030000	18530 HOFF RD		
BERNHARD FAMILY LAND		ELWOOD IL 60421		
TRUST	1011262000040000	18856 W HOFF RD		
BERNHARD LAND TRUST T-		ELWOOD IL 60421		
98				
C/O LEO & GREG	1011263000040000	18530 HOFF RD		
BERNHARD		ELWOOD IL 60421		
NORMA L BAUDINO		2600 GRAND AVE		
BAUDINO LAW FIRM	1011264000010000	STE 300		
		DES MOINES IA		
		50312		
SEWING HEINZ A ETAL	1011271000030000	26601 S CHICAGO		
		RD		
		ELWOOD IL 60421		
BERNHARD LAND TRUST T-				
98				
C/O LEO & GREG	1011272000050000	18530 HOFF RD		
BERNHARD		ELWOOD IL 60421		
BERNHARD LEO E LOIS I	1011273000020000	18530 W HOFF RD		
		ELWOOD IL 60421		
BERNHARD FARMS	1011273000030000	18530 W HOFF RD		
		ELWOOD IL 60421		
BERNHARD FARMS INC	1011274000010000	18530 W HOFF RD		
		ELWOOD IL 60421		
HARTZ CONSTRUCTION TR		9026 HERITAGE		
HBT 1939	1011281000020000	PKWY		
		WOODRIDGE IL		
		60517		
COLDWATER LAVERN A		26845 COLDWATER		
KATHLEEN	1011281000030000	RD		
		ELWOOD IL 60421		

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Phase I Rezoning - Parcel Proximity List

Property: **Compass Business Park**
Elwood, Illinois

JHA Project #: F345

Date: 9/6/2017

Entity/Owner	PIN	Address	Northpoint Notes	JHA Notes
SHEWMAKE DONALD L DEBRA K	1011281000050000	PO BOX 283 19820 W MISSISSIPPI AVE ELWOOD IL 60421		
BRAUN MARY B REV DEV TRUST	1011281000080000	117 W SPENCER ST ELWOOD IL 60421		
RENA M. BRYAN	1011281000110000	19948 MISSISSIPPI AVE ELWOOD IL 60421		
JEFF SHACKELFORD	1011281000120000	PO BOX 382 ELWOOD IL 60421		
PHILIP L. CARLOS AND PATRICIA A. CARLOS, HUSBAND AND WIFE AS JOINT TENANTS	1011281010010000	19828 MISSISSIPPI AVE ELWOOD IL 60421		
KENNETH SCHROEDER AND DONNA SCHROEDER, HUSBAND AND WIFE, AS JOINT TENANTS	1011281010020000	24203 S SUSAN LN MANHATTAN IL 60442		
COLDWATER LAV A KATH	1011282000010000	26845 COLDWATER RD ELWOOD IL 60421		
COLDWATER FARMS	1011282000020000	26845 COLDWATER RD ELWOOD IL 60421		
COLDWATER LAV A KATH	1011283000010000	26845 COLDWATER RD ELWOOD IL 60421		
BERNHARD FARMS INC	1011284000010000	18530 HOFF RD ELWOOD, IL 60421		
HARTZ CONSTRUCTION CO INC TR HBT-1939	1011292000110000	9026 HERITAGE PKWY WOODRIDGE IL 60517		
LB ANDERSEN LAND HOLDING, LLC	1011292070010000	104 S WYNSTONE PARK DR NORTH BARRINGTON IL 60010		
LB ANDERSEN LAND HOLDING, LLC	1011292070030000	104 S WYNSTONE PARK DR NORTH BARRINGTON IL 60010		
CENTERPOINT INTERMODAL CENTER EAST PROPERTY OWNERS ASSOCIATION	1011292070040000	1808 SWIFT DR OAK BROOK IL 60523		
BISSELL MIDWEST DC LEASING, LLC	1011294010020000	2345 WALKER RD NW GRAND RAPIDS MI 49544		

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[Handwritten Signature]



Phase I Rezoning - Parcel Proximity List

Property: **Compass Business Park**
Elwood, Illinois

JHA Project #: F345

Date: 9/6/2017

Entity/Owner	PIN	Address	Northpoint Notes	JHA Notes
LB ANDERSEN LAND HOLDING, LLC	1011294010030000	104 S WYNSTONE PARK DR NORTH BARRINGTON IL 60010		
EGLPC BROWN PROPERTY LLC C/O NORTHPOINT DEVELOPMENT LLC	1011224000070000*	4825 NW 41ST ST STE 500 RIVERSIDE MO 64150		
EGLPC BROWN PROPERTY LLC C/O NORTHPOINT DEVELOPMENT LLC	1011224000080000*	4825 NW 41ST ST STE 500 RIVERSIDE MO 64150		
EGLPC BROWN PROPERTY LLC C/O NORTHPOINT DEVELOPMENT LLC	1011224000100000*	4825 NW 41ST ST STE 500 RIVERSIDE MO 64150		
WILLIAMS-RIEGEL PROPERTY LLC C/O WILLIAMS JEANNE A	1011233000010000*	1832 EAGLE CREEK RD ELWOOD IL 60421		
WALDEN DONALD D SR	1011261000020000*	622 BETHEL DR JOLIET IL 60435		
WALDEN DONALD D SR	1011261000030000*	622 BETHEL DR JOLIET IL 60435		
WALDEN DONALD JR	1011261000040000*	26319 S ROWELL RD ELWOOD IL 60421		
STEFFES ROBERT E FMLY PTSHP LP	1011271000050000*	16211 W DELANEY RD MANHATTAN IL 60442		
STEFFES ROBERT E FMLY PTSHP LP	1011271000070000*	16211 W DELANEY RD MANHATTAN IL 60442		
LORI J. STEFFES, AS TRUSTEE OF THE LORI J. STEFFES TRUST DATED NOVEMBER 18, 2013	1011271000080000*	16211 W DELANEY RD MANHATTAN IL 60442		
LB ANDERSEN LAND HOLDING, LLC	1011294010010000*	104 S WYNSTONE PARK DR NORTH BARRINGTON IL 60010		

*Parcel is within proposed rezoning limits

Notes:

1. Permanent Index Numbers (also referred to as PINs or Parcel IDs) have been obtained from the Will County GIS Data Viewer on September 5, 2017.
2. Owner name and address information has been obtained from the 2016 Levy Real Estate Tax Information available from the Will County Treasurer.

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[Signature]



Phase I Rezoning - Parcel Proximity List

Property: **Compass Business Park**
Elwood, Illinois

JHA Project #: F345

Date: 11/28/2017

Entity/Owner	PIN	Address	Northpoint Notes	JHA Notes
WALSH LAWRENCE M IRENE	1011224000090000	18801 W BROWN RD ELWOOD IL 60421		
SIEVERT MICHAEL L TRUST	1011224000110000	18655 W BROWN RD ELWOOD IL 60421		
ARNHOLD WALTER J DEC TRUST	1011231000030000	24212 S ROWELL RD ELWOOD IL 60421		
ARNHOLD CYNTHIA L DEC TRUST				
CHICAGO TITLE LAND TRUST CO	1011232000020000	10 S LASALLE ST STE 2750 CHICAGO IL 60603		
TR 8002367519		26057 S RIDGE RD ELWOOD IL 60421		
DART INC TR 183	1011234000040000	26057 S RIDGE RD ELWOOD IL 60421		
C/O ROBERT C SWEEDLER	1011234000050000	18530 HOFF RD ELWOOD IL 60421		
DART INC C/O ROBERT SWEEDLER		18856 W HOFF RD ELWOOD IL 60421		
BERNHARD FARMS INC	1011262000030000	18530 HOFF RD ELWOOD IL 60421		
BERNHARD FAMILY LAND TRUST	1011262000040000	18530 HOFF RD ELWOOD IL 60421		
BERNHARD LAND TRUST T- 98	1011263000040000	18530 HOFF RD ELWOOD IL 60421		
C/O LEO & GREG		2600 GRAND AVE STE 300 DES MOINES IA 50312		
NORMA L BAUDINO BAUDINO LAW FIRM	1011264000010000	26601 S CHICAGO RD ELWOOD IL 60421		
SEWING HEINZ A ETAL	1011271000030000	18530 HOFF RD ELWOOD IL 60421		
BERNHARD LAND TRUST T- 98	1011272000050000	18530 HOFF RD ELWOOD IL 60421		
C/O LEO & GREG		18530 W HOFF RD ELWOOD IL 60421		
BERNHARD LEO E LOIS I	1011273000020000	18530 W HOFF RD ELWOOD IL 60421		
BERNHARD FARMS	1011273000030000	18530 W HOFF RD ELWOOD IL 60421		
BERNHARD FARMS INC	1011274000010000	18530 W HOFF RD ELWOOD IL 60421		
HARTZ CONSTRUCTION TR HBT 1939	1011281000020000	9026 HERITAGE PKWY WOODRIDGE IL 60517		
COLDWATER LAVERN A KATHLEEN	1011281000030000	26845 COLDWATER RD ELWOOD IL 60421		
SHEWMAKE DONALD L DEBRA K	1011281000050000	PO BOX 283 19820 W MISSISSIPPI AVE ELWOOD IL 60421		
BRAUN MARY B REV DEV TRUST	1011281000080000	117 W SPENCER ST ELWOOD IL 60421 19948 MISSISSIPPI		
RENA M. BRYAN	1011281000110000	AVE ELWOOD IL 60421 PO BOX 382 ELWOOD IL 60421		
JEFF SHACKELFORD	1011281000120000	19828 MISSISSIPPI AVE ELWOOD IL 60421		
PHILIP L. CARLOS AND PATRICIA A. CARLOS, HUSBAND AND WIFE AS JOINT TENANTS	1011281010010000			



Phase I Rezoning - Parcel Proximity List

Property: **Compass Business Park**
Elwood, Illinois

JHA Project #: F345

Date: 11/28/2017

Entity/Owner	PIN	Address	Northpoint Notes	JHA Notes
KENNETH SCHROEDER AND DONNA SCHROEDER, HUSBAND AND WIFE, AS JOINT TENANTS	1011281010020000	24203 S SUSAN LN MANHATTAN IL 60442		
COLDWATER LAV A KATH	1011282000010000	26845 COLDWATER RD ELWOOD IL 60421		
COLDWATER FARMS	1011282000020000	26845 COLDWATER RD ELWOOD IL 60421		
COLDWATER LAV A KATH	1011283000010000	26845 COLDWATER RD ELWOOD IL 60421		
BERNHARD FARMS INC	1011284000010000	18530 HOFF RD ELWOOD, IL 60421		
GRUNDY COUNTY NATL BANK	1011291360060000	TR 1304 PO BOX 520 MORRIS IL 60450		
GRUNDY COUNTY NATL BANK	1011291400010000	TR 1304 PO BOX 520 MORRIS IL 60450		
HARTZ CONSTRUCTION CO INC TR HBT-1939	1011292000110000	9026 HERITAGE PKWY WOODRIDGE IL 60517		
LB ANDERSEN LAND HOLDING, LLC	1011292070010000	104 S WYNSTONE PARK DR NORTH BARRINGTON IL		
LB ANDERSEN LAND HOLDING, LLC	1011292070020000	104 S WYNSTONE PARK DR NORTH BARRINGTON IL		
GRUNDY COUNTY NATL BANK	1011293000030000	TR 1304 PO BOX 520 MORRIS IL 60450		
GRUNDY COUNTY NATL BANK	1011293000090000	TR 1304 PO BOX 520 MORRIS IL 60450		
ATG ILLINOIS LAND TRUST	1011293000150000	TR L006-027 PO BOX 90 ELWOOD IL 60421		
ATG ILLINOIS LAND TRUST	1011293000160000	TR L006-027 PO BOX 90 ELWOOD IL 60421		
CENTERPOINT INTERMODAL CENTER EAST PROPERTY OWNERS ASSOCIATION/S PURCELL	1011293020010000	1808 SWIFT DR OAK BROOK IL 60523		
ILLINOIS STATE POLICE ATTN GREG KILDUFF	1011294000050000	801 S 7TH ST STE 300 S SPRINGFIELD IL 62703		
ZUBEK STANLEY THERESA	1011294000070000	20002 W HOFF RD ELWOOD IL 60421		
LB ANDERSEN LAND HOLDING, LLC	1011294000100000	104 S WYNSTONE PARK DR NORTH BARRINGTON IL		



Phase I Rezoning - Parcel Proximity List

Property: **Compass Business Park**
Elwood, Illinois

JHA Project #: F345

Date: 11/28/2017

Entity/Owner	PIN	Address	Northpoint Notes	JHA Notes
JOHNSON LARRY D JOYCE E TR	1011294000110000	27000 S COLDWATER RD ELWOOD IL 60421		
LB ANDERSEN LAND HOLDING, LLC	1011294000120000	104 S WYNSTONE PARK DR NORTH BARRINGTON IL		
BISSELL MIDWEST DC LEASING, LLC	1011294010020000	2345 WALKER RD NW GRAND RAPIDS MI 49544		
USA	1011321000010000			Will County Treasurer does not have an address on file - per USDA website: 30239 S STATE ROUTE 53 WILMINGTON, IL 60481
USA	1011322000010000			""
USA	1011322000020000			""
EGLPC BROWN PROPERTY LLC C/O NORTHPOINT DEVELOPMENT LLC EGLPC BROWN PROPERTY LLC C/O NORTHPOINT DEVELOPMENT LLC EGLPC BROWN PROPERTY LLC C/O NORTHPOINT DEVELOPMENT LLC	1011224000070000*	4825 NW 41ST ST STE 500 RIVERSIDE MO 64150		
WILLIAMS-RIEGEL PROPERTY LLC C/O WILLIAMS JEANNE A	1011233000010000*	4825 NW 41ST ST STE 500 RIVERSIDE MO 64150		
WALDEN DONALD D SR	1011261000020000*	832 EAGLE CREEK RD ELWOOD IL 60421		
WALDEN DONALD D SR	1011261000030000*	622 BETHEL DR JOLIET IL 60435		
WALDEN DONALD JR	1011261000040000*	622 BETHEL DR JOLIET IL 60435		
STEFFES ROBERT E FMLY PTSHP LP	1011271000050000*	26319 S ROWELL RD ELWOOD IL 60421		
STEFFES ROBERT E FMLY PTSHP LP	1011271000070000*	16211 W DELANEY RD MANHATTAN IL		
LORI J. STEFFES, AS TRUSTEE OF THE LORI J. STEFFES TRUST DATED NOVEMBER 18, 2013	1011271000080000*	16211 W DELANEY RD MANHATTAN IL 60442		
LB ANDERSEN LAND HOLDING, LLC	1011292070030000*	104 S WYNSTONE PARK DR NORTH BARRINGTON IL		
CENTERPOINT INTERMODAL CENTER EAST PROPERTY OWNERS ASSOCIATION/S PURCELL	1011292070040000*	1808 SWIFT DR OAK BROOK IL 60523		



Phase I Rezoning - Parcel Proximity List

Property: **Compass Business Park**
Elwood, Illinois

JHA Project #: F345

Date: 11/28/2017

Entity/Owner	PIN	Address	Northpoint Notes	JHA Notes
LB ANDERSEN LAND HOLDING, LLC	1011294010010000*	104 S WYNSTONE PARK DR NORTH BARRINGTON IL		
LB ANDERSEN LAND HOLDING, LLC	1011294010030000*	104 S WYNSTONE PARK DR NORTH BARRINGTON IL		

*Parcel is within proposed rezoning limits

Notes:

1. Permanent Index Numbers (also referred to as PINs or Parcel IDs) have been obtained from the Will County GIS Data Viewer on September 5, 2017.
2. Owner name and address information has been obtained from the 2016 Levy Real Estate Tax Information available from the Will County Treasurer.

GRAPHIC SCALE 1" = 1,000'

NOTE: PARCEL LINE INFORMATION HAS BEEN OBTAINED FROM 2016 TAX PARCEL VECTOR DATA AVAILABLE FROM THE WILL COUNTY GIS SYSTEM.

**250' PROXIMITY PARCEL EXHIBIT
REZONING APPLICATION NOTIFICATION RECIPIENTS**



Village of Elwood

• Established 1889 •

Proud of our past, Building our future.

401 E. Mississippi Ave., P.O. Box 435, Elwood, IL 60421 • (815) 423-5011 • Fax: (815) 423-6861



200812020142545

R2008142545

Receipt # T20080128777

Karen A. Stukel Will County Recorder 59P

LH Date 12/02/2008

Time 13:42:11

Recording Fees:

\$79.75

IL Rental Hsng. Support Program:

\$0.00

CERTIFICATION

I, PATRICIA BUCHENAU, do hereby certify that I am the keeper of the Ordinances of said Municipality; and that the attached and foregoing is a true, correct and complete copy of Ordinance 923 passed by the Board of Trustees of said Municipality as appears from the records of said Municipality. Ordinance was approved October 15, 2008.

An Ordinance Adopting an Official Comprehensive Plan for the Village of Elwood, Will County, Illinois

IN WITNESS WHEREOF, I have hereunto set my hand as Village Clerk of said Municipality and have hereunto affixed the seal thereof this 21st day of November 2008.



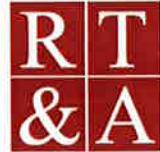
Patricia Buchenau
Village Clerk

VILLAGE OF ELWOOD COMPREHENSIVE PLAN



Village of Elwood

OCTOBER, 2008



RUETTIGER, TONELLI & ASSOCIATES, INC.

ORDINANCE 923

AN ORDINANCE ADOPTING AN OFFICIAL COMPREHENSIVE PLAN FOR THE VILLAGE
OF ELWOOD, WILL COUNTY, ILLINOIS

PASSED AND APPROVED BY
THE PRESIDENT AND BOARD OF TRUSTEES
THE 15 DAY OF October, 2008

Published in pamphlet form by
authority of the corporate authorities
of the Village of Elwood, Illinois,
the 17 day of Oct., 2008.

ORDINANCE NO. 923

AN ORDINANCE ADOPTING AN OFFICIAL COMPREHENSIVE PLAN FOR THE VILLAGE OF ELWOOD, WILL COUNTY, ILLINOIS

WHEREAS, the Village of Elwood is a Home Rule Unit under subsection (a) of Section 6 of Article VII of the Constitution of the State of Illinois of 1970; and

WHEREAS, a Home Rule Unit may exercise any power and perform any function pertaining to its government and affairs for the protection of the public health, safety, morals and welfare; and

WHEREAS, pursuant to notice published in a newspaper of general circulation within the Village, the duly appointed members of the Plan Commission of the Village of Elwood have heretofore conducted a public hearing to review, evaluate and analyze an official comprehensive plan for the present and future development of the Village; and

WHEREAS, the Plan Commission has made its final report and recommended to the Acting Village President and Board of Trustees ("Corporate Authorities") that the comprehensive plan be approved for the entire municipality; and

WHEREAS, the Corporate Authorities have received the Plan Commission's final report and recommendation within the time provided by law, and pursuant to state statutes 65 ILCS 5/11-12-7 (2008); and

WHEREAS, the corporate authorities for the Village of Elwood have conducted a public hearing thereon pursuant to notice as provided by law, and pursuant to state statutes 65 ILCS 5/11-12-7 (2008) and otherwise deliberated the merits of the Comprehensive Plan following the public hearing.

NOW, THEREFORE, BE IT ORDAINED BY THE ACTING VILLAGE PRESIDENT AND BOARD OF TRUSTEES OF THE VILLAGE OF ELWOOD, WILL COUNTY, ILLINOIS, PURSUANT TO ITS HOME RULE AND STATUTORY AUTHORITY AS FOLLOWS:

SECTION 1 INCORPORATION That the preambles to this Ordinance be, and the same hereby are incorporated herein by this reference as if set out in full at this place.

SECTION 2 ADOPTION OF PLAN The proposed Comprehensive Plan attached hereto and incorporated herewith as Exhibit "A" is hereby adopted as the Official Comprehensive Plan of the Village of Elwood, Will County, Illinois.

SECTION 3 ADOPTION OF MAP The proposed map attached hereto as Exhibit "B" and incorporated herein by reference be is hereby adopted as the Official Map of the Village of Elwood.

SECTION 4 RECORDING The Village Clerk is hereby authorized and directed to record with the Will County Recorder of Deeds notice of the adoption of the Official Comprehensive Plan as provided by law and pursuant to state statute 65 ILCS 5/11-12-7.

SECTION 5 PUBLICATION The Village Clerk is hereby authorized and directed to place the Official Comprehensive Plan and Official Map on file and shall make them available at all times during business hours for public inspection as provided by law, and pursuant to state statutes 65 ILCS 5-11-12-6.

SECTION 6 REPEALER All Ordinances and/or Ordinances approved by the Village of Elwood which are inconsistent herewith, to the extent of such inconsistency and no further, are hereby repealed upon the effective date hereof.

SECTION 7 SEVERABILITY Should any section, subsection or other provision of this Ordinance for any reason be held invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not effect the validity of the Ordinance as a whole or any part not declared invalid.

SECTION 8 EFFECTIVE DATE That this Ordinance shall be in full force and effect from and after its passage, approval, and publication in pamphlet form as provided by law, but in no event earlier than ten (10) days after filing of a notice of its adoption with the Will County Recorder of Deeds.

AYES: 6
NAYS: 0
ABSENT: 0

Trustee	Aye	Nay	Absent	Trustee	Aye	Nay	Absent
Charles Bernhard	X			Donald LaPaglia	X		
Francis "Bud" Jones	X			Mary Matichak	X		
Jason Kucera	X			Robert Wagner	X		

PASSED and APPROVED this 15 day of October, 2008.


WALTER STRAWN,
ACTING VILLAGE PRESIDENT

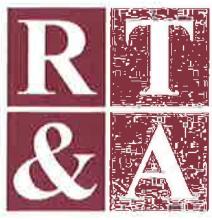
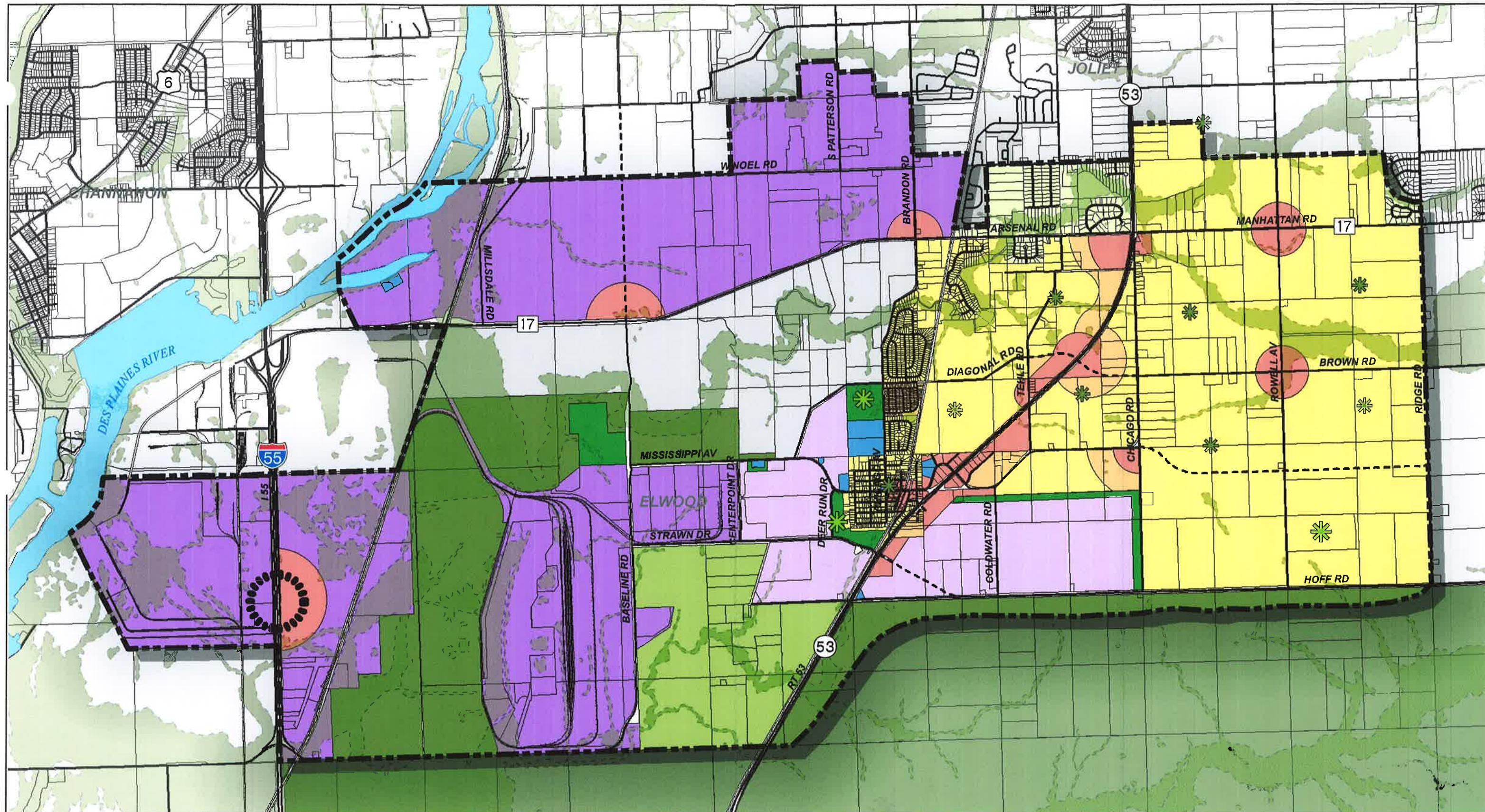
ATTEST:


VILLAGE CLERK

PASSED: This 15 day of October, 2008.

APPROVED: This 15 day of October, 2008.

PUBLISHED: This 17 day of October, 2008.



Legend

- Planning Area
- Proposed Interchange
- Existing Road
- Proposed Road

Proposed Land Use

- Rural Residential
- Single-Family Residential
- Multi-Family Residential
- Commercial
- Light Industrial
- Industrial
- Governmental/Institutional

- Park/Open Space
- Midewin National Tall Grass Prairie
- Abraham Lincoln National Cemetery
- Joliet Army Training Area

- Environmentally Sensitive Area
- Parks Plan
- Community Park
- Neighborhood Park

N
W E
S
October 2008

Comprehensive Plan
Proposed Land Use Map
Illustration 6



Village of Elwood

ACKNOWLEDGEMENTS

Village President Pro Temp

Walter Strawn

In Memoriam

Janice Ruban

Board of Trustees

Francis "Bud" Jones

Charles Bernhard

Jason Kucera

Mary Matichak

Donald LaPaglia

Planning and Zoning Commission

Chairman Doug Jenco

Jim Bosonetta

Floyd Garret

Greg Hickey

Lenny Pulaski

Stella Weisfus

Wes Winkler

Clerk

Patricia Buchenau

Treasurer

Roberta Day

Village Administrator

Aimee Ingalls

Consultant

Ruettiger, Tonelli & Associates, Inc.

2174 Oneida Street

Joliet, IL 60435

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INTRODUCTION

The Village of Elwood is a small, but growing community located approximately five miles south of the City of Joliet in Will County, Illinois. The Village is rapidly becoming a part of the urban fringe of Chicago, as Will County continues to maintain its position as the one of the fastest growing county in Illinois. The Villages relative location near Interstates 55 and Interstate 80, the development of the former Joliet Arsenal, the planned infrastructure improvements, in addition to the many developments in the Will County region make Elwood a strong candidate for significant growth in the near future.

The Need for a Comprehensive Plan

The Village of Elwood last updated its plan in 2003. Based on the 2000 census the population of the Village was 1,620 people. Population estimate from the Census Bureau for 2006 is 2,300, an increase of 42% in 6 years. The Chicago Metropolitan Planning Agency (CMAP) estimates the population of the Village to reach 19,188 people by the year 2030. This estimate represents an 834% increase in population in under twenty-five years.

The Village of Elwood has benefited from the rapid growth in Will County and will continue to do so in the foreseeable future. The decommissioning of the former Joliet Arsenal and its release of its public lands to private sector interests are having a profound effect on the Village of Elwood and will continue a positive trend of growth. The buildup of the Centerpoint Intermodal Center Elwood and construction of the East expansion have been essential elements in driving growth in the Village. The approval by Joliet of plans for a large intermodal center and industrial park just north of the Elwood boundary agreement with Joliet will provide for future growth opportunities. While the development of the former Arsenal is already proceeding and future intermodal in Joliet is beginning, the potential construction of the proposed South Suburban airport near Peotone is not. However, this potential development must be considered in the drafting of long-range plans for the Village. Inevitably, the Village will continue to experience development pressures. The Village is in the path of development as its neighbors to the north and west have been experiencing unprecedented growth. As a result, the need for comprehensive planning is as strong as ever as the Village prepares for future opportunities. The Village has chosen a pro-active approach in determining the future course of the Village in the development of this Comprehensive Plan.

This Comprehensive Plan has several legislative uses that are important to understand:

1. The Comprehensive Plan is a set of goals and objectives that are aimed at guiding the future development of the Village.
2. The Comprehensive Plan serves as a vital tool in assisting the Plan Commission and Village Board in the decision-making processes that each of those bodies face. The Comprehensive Plan should help guide decisions regarding such development issues as subdivisions, annexations, map amendments and development agreements.
3. The Comprehensive Plan is the primary tool from which the Plan Commission and its advisors may draw from in addressing the growth pressures facing the Village of Elwood.
4. The Comprehensive Plan clearly provides a unified set of goals and objectives to all of the stakeholders involved in the development process in the Village of Elwood. These parties include the Village Board, Plan Commission, Village staff, outside governmental agencies, developers, the courts, and the general public.
5. The Comprehensive Plan is an educational resource that outlines the goals, objectives, development plans and resources associated with the Village of Elwood.

Village Location

The Village of Elwood is located in southwest Will County, Illinois approximately 45 miles southwest of Chicago. The City is situated south of the City of Joliet and east of Interstate 55. The redevelopment of the Joliet Arsenal into the Centerpoint Intermodal Center along with access to the surrounding region through Illinois Route 53, I-55 and I-80 create great opportunities for growth. The proximity to the Midewin National Tallgrass Prairie provides for exceptional recreational and outdoor activities.

Figure 1: Location Map



Description of the Planning Area

The Comprehensive Plan will set forth the development guidelines and implementation strategies for the existing corporate limits of the Village of Elwood, its 1 ½ mile planning jurisdiction and areas outside these jurisdiction that represent strategic planning areas.

The Comprehensive Plan defines a planning area that encompasses the Village of Elwood and surrounding areas to identify future development patterns and objectives. The planning boundary defines the area to be studied within the Comprehensive Plan.

The planning area includes the existing corporate limits of the Village of Elwood, its 1 ½ mile planning jurisdictional area, and areas outside this jurisdiction that can be reasonably developed within the Village. The planning area is generally bound by Ridge Road to the east, Midewin National Tallgrass Prairie to the south, Des Plaines River to the west and the Joliet boundary agreement to the north and contains approximately 29 square miles.

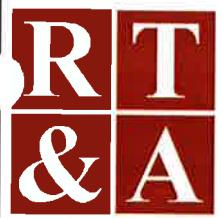
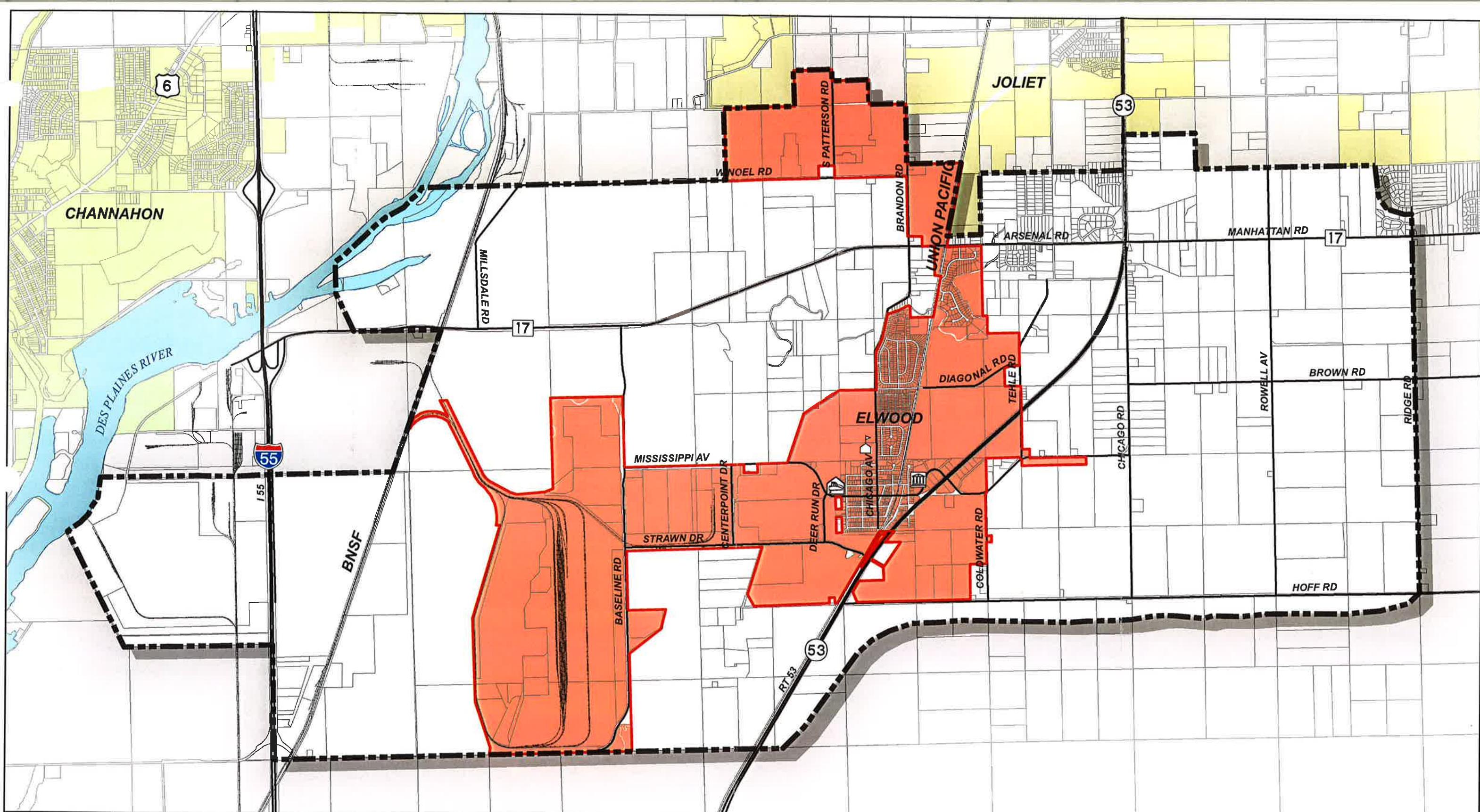
-Planning Area shown on Illustration 1: Planning Area Map.

Planning Sectors

The Plan has four primary planning sectors within the overall planning area and also contains four types of node developments that will give general guidelines for future growth (Comprehensive Plan Map).

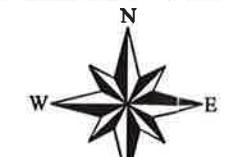
1. The first planning sector is designated as the “Traditional Town”. This area is what most residents would consider the original town and extension of Mississippi Avenue. The original town center was made up of slightly more than 160 acres that comprised the Village until the addition of the lands that resulted from the annexation of the Elwood Energy and Peoples Natural Gas Light and Coke company’s properties. This area represents a large portion of the Village of Elwood’s existing housing stock. The intent and purpose of this sector is to provide guidelines and strategies that are aimed at improving the quality of life for the Village’s residents through long-term infrastructure improvements, downtown redevelopment and streetscape improvements while maintaining the unique historic and traditional character of the sector.
2. The second planning sector is the “Suburban Corridor Sector”. This area is comprised of those properties located within the immediate vicinity of Illinois Route 53 and areas to the east planning boundary. It is the intent and purpose of this sector to serve as the Village’s primary commercial center, interspersed with suburban density residential uses that are essential to assure diversity in the Village’s long-term housing needs.

3. The fourth planning sector is designated as the “Northwest Industrial Sector”. This area roughly comprises the properties north of Arsenal Road, west of the railroad tracks to the Des Plaines River and currently includes Elwood Energy, Peoples Gas Light and Coke Company and the Stepan Corporation. The intent and purpose of this area is to encourage and promote heavy industrial uses that are characteristic of those permitted and special uses that comprise the “I-4” Heavy Industrial District found within the Village of Elwood Zoning Ordinance as well as an area of light industrial use to buffer the sector from existing and planned residential uses.
4. The fifth planning sector is the “Southwest Industrial Sector”. This area is comprised of those properties that are located in the south and southwest portions of the planning area and are heavily influenced by the development of the Centerpoint Intermodal Center Elwood and east expansion. The intent and purpose of this sector is to provide an area of mixed uses that include light industrial, commercial and open space land uses that reflect a high degree of creativity in design and environmental quality given this areas location adjacent to the Village’s residential areas. This sector also incorporates existing and planned heavy industrial uses consistent with the Village’s “I-4” District.



Legend

- Planning Area
- Municipal Boundaries
- Village of Elwood
- Village of Channahon
- City of Joliet



October 2008

Comprehensive Plan

Planning Area

Illustration 1



Village of Elwood

COMMUNITY PROFILE

Historical Perspective

The early settlement of Elwood and Jackson Township occurred in 1854 upon completion of the then Chicago and Mississippi railroad whose line connected Joliet and Bloomington. The town was surveyed (Figure 1) and lots were offered for sale in late 1854 and 1855 by three gentleman named Spencer, Gardner and Myers. The area surrounding the railroad soon prospered with many businesses locating along what is today Mississippi Street. Original businesses included a blacksmith shop, drug store, hotel and post office. In addition to the merchants who located on Mississippi Street, Elwood was once home to a brick and tile manufacturer and a cheese factory.

The Village was formerly incorporated in 1869 with William Muhlig serving as its first President. In 1873, the Village was reorganized under State legislation. John H. Bridge served as President of the Village under the new system, along with five trustees, a clerk and a treasurer created under the system. This form of government, with minor revision, remains in effect today.

On the night of May 28, 1874, a fire swept through the business district of the Village destroying all but one store and the hotel. The fire stunted the once bustling Village's growth, as property losses totaled \$30,000, with not more than \$1,000 being insured. Many proprietors were permanently put out of business as a result of the fire and the Village suffered deeply as a result.

Elwood remained a small farming and railroad town throughout the late 1800s and early 1900s with its population remaining steady at approximately 400 people. Shortly before World War II, the federal government acquired several thousand acres of property for the construction of a munitions facility. Once complete, the facility totaled 23,000 acres. The facility at its peak, employed several thousand people, including many residents of the Village of Elwood.

The population grew to nearly 800 people by 1970. The Village continued to experience very moderate growth through the 1970s and 1980s, reaching a population of 951 people in 1990. In 1993, the Arsenal was closed, but the Village did not look back. By 2000, the population of the Village had swelled to 1,620, an increase of 70% from 1990. This era of unprecedented growth for the Village appears to be continuing as the Village prepares for new development. The 2006 population is estimated to be 2,300 people. This figure represents an increase of 142% over 1990 totals and over 42% during the six year time period since 2000.

There are signs that growth in the Village will slow during the current housing slowdown. The Village's location relative to Interstates 55 and 80 allow its residents to easily travel to employment centers in Joliet, Chicago and the southwest suburbs. As Will County continues to hold its position as one of the fastest growing Counties in Illinois, the Village of Elwood will continue to benefit. The addition of the NASCAR/NHRA facilities to the north and the redevelopment of the Arsenal, are just two of the many developments either in or near the Village, which will have long-term effects on the Village's expansion. The Village now has the opportunity to effectively plan to assure an environment of balanced growth that includes a high quality of life for its residents while creating the necessary tax base that commercial and industrial development provide.

Background Information

Demographic and economic data are important in determining an area's growth potential. Trends can be identified that may be able to identify a community's future planning needs. For example, if population data indicates a large conglomeration of residents of a community in the upper age categories (50+ / also indicated by an increasing median age), it would likely be prudent to begin planning for the future needs of the aging population, such as health care services and senior housing. On the other hand, if there are an above average number of persons age six and under, it would be wise for school districts to develop plans for the construction of a new school or the addition of more teachers. The following section describes the demographics of the Village of Elwood and the surrounding areas with a brief analysis of these statistics.

Population Growth Pattern

The population of the Village of Elwood saw a large amount of growth from 1940 to 1960 increasing by almost 70-80% each decade going from a population of 248 in 1940 to 746 in 1960. The Village has grown since 1970 when it stood at a population of 794. Between 1970 and 1990, the Village grew by 157 people to 951, an increase of 17%. Between 1990 and 2006, the Village increased by 1,349 persons to a total of 2,300 persons an increase of 142% (Table 1).

Table 1: Population Growth

Year	Population	% Change from Previous
1940	248	-
1950	420	69.4%
1960	746	77.6%
1970	794	6.4%
1980	814	2.5%
1990	951	16.8%
2000	1,620	70.3%
2006*	2,300	42.0%
2030**	19,188	734.3%

Sources: U.S. Census Bureau, Census 2000

*U.S. Census Bureau, July 2006 Population Estimates

**NIPC 2030 Forecasts (September 27, 2006)

The Village's exceptional growth is primarily a result of the opening of new employment centers in Elwood, Joliet, Will County, southern DuPage County and the southwest suburbs. From 1990 to 2006 Will counties population has increased by 310,904 people to a total of 668,217 an increase of 87%. The relative ease of commute with Interstate 80 and Interstate 55 in the near vicinity makes the Village an ideal place of residence for those who work in these new employment centers.

The availability of land in Elwood will play important factor in the Village's population growth. As will the employment centers in Elwood and the surrounding municipalities such as the Centerpoint Intermodal Center, the Centerpoint East expansion and the approved Centerpoint Intermodal Center Joliet. The addition of the proposed South Suburban Airport could also have significant impacts on the Village's growth. The Northeastern Illinois Planning Commission (NIPC) has estimated that the population of Elwood could exceed 19,188 people by 2030.

As the Chicago metropolitan region continues to grow, the Village will be a beneficiary. Though there will likely be ups and downs in the local, regional and national economies, which will mean periods of rapid growth or recession, the Village of Elwood is well positioned to continue its current experience of moderate and controlled growth well into the future.

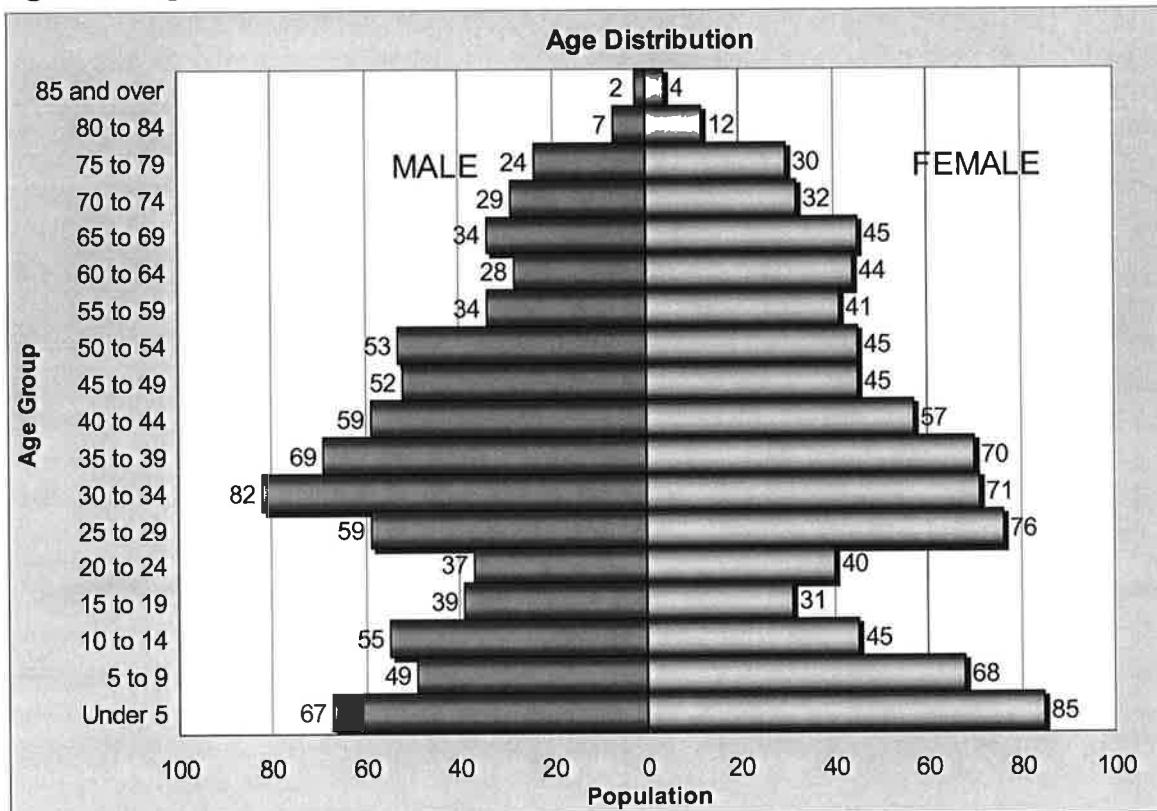
Age Distribution

The median age of a resident of the Village of Elwood in 2000 was 35.2 up from the 29.7 average of 1990 (Table 2). The Village of Elwood has a relatively young population but is seeing a slight increase in average age. There is a large increase in population 45 to 64 with almost 120% increase and a 170% increase in population over 65. The increase in the population from 45 to 65 and 65 can result in much larger needs for care, transportation, senior and medical services.

Table 2: Population Age Group

Age group	1990	2000	Percent Change
0 to 4	95	152	60.0%
5 to 17	199	257	29.1%
18 to 24	102	107	4.9%
25 to 44	318	543	70.8%
45 to 64	156	342	119.2%
65 and over	81	219	170.4%
Total	951	1,620	70.3%
Median	29.7	35.2	

Sources: U.S. Census Bureau, Census 1990, 2000

Figure 2: Population Age Distribution

Sources: U.S. Census Bureau, Census 2000

Households and Housing Units

The total number of households in the Village of Elwood in 1990 was 352 out of a total of 362 available housing units. This rose sharply to 637 total households of a total of 675 total units in 2000. At the same time, the average number of persons per household dropped from 2.7 in 1990 to 2.53 in 2000 (Table 3).

Table 3: Household Size

Persons in Household	Number
1 person	141
2 persons	236
3 persons	113
4 persons	91
5 persons	39
6 persons	12
7 persons or greater	5
Total Households	637

Sources: U.S. Census Bureau, Census 2000

Housing Stock

The Village had a total of 675 housing units in 2000. Since 2000, the Village has permitted the construction of 256 residential units, bringing the total number of housing units to 745 (Table 4). After evaluation of population, households and building permits, it becomes clear that nearly all of the population growth occurring results from migration to the Village of Elwood, rather than any changes in the birth/mortality cycle.

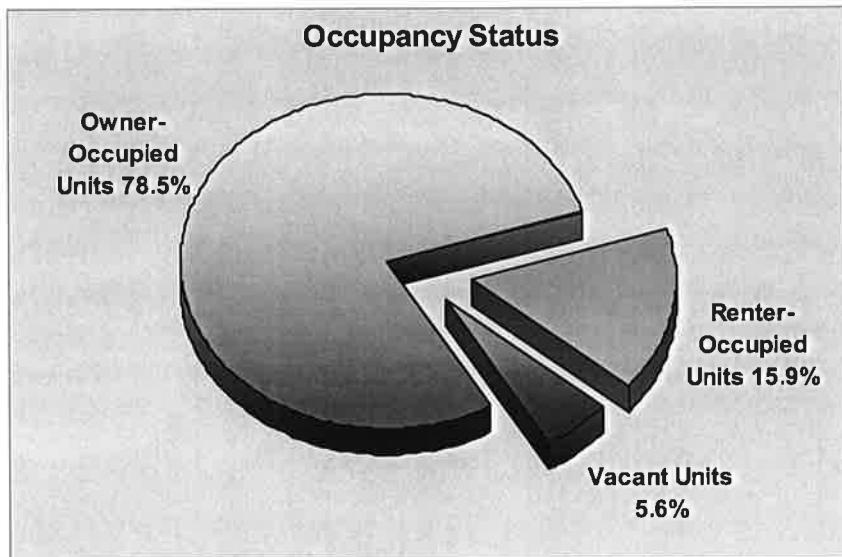
Table 4: Building Permits

Year	Units Built
2000	50
2001	48
2002	22
2003	46
2004	32
2005	19
2006	33
2007	6
Total	256

Sources: U.S. Census Bureau

A total of 637 units of the Village's 675 available residential units in 2000 where occupied. Of the total units 530 (78.5%) were owner occupied, 107 (15.9%) where renter occupied units and 38 (5.6%) where vacant units. This ratio shows an increase in owner occupied housing since it was at 70.4% in 1990. With a 94.4% occupancy rate and ratio of persons per household decreasing, new housing stock will need to be added for future population growth.

Figure 3: Housing Occupancy Status



Sources: U.S. Census Bureau, Census 2000

Elwood contains a variety of housing types including single-family, attached single-family, and multi-family. Housing costs vary, thus making Elwood affordable for families in a wide range of economic levels. Great diversity appears in the age of the housing stock; Elwood boasts brand new housing and a core of beautiful older homes. The Village contains predominantly (61.4%) single-family detached units with some (22.5%) single-family attached (duplex) units.

Table 5: Housing Type

Housing Type	Number
1-unit, detached	416
1-unit, attached	153
2 units	22
3-4 units	26
5-9 units	7
10 to 19 units	0
20 or more units	0
Mobile home	54
Total	678

Sources: U.S. Census Bureau, Census 2000

Income

In 2000, the median household income in the Village of Elwood was \$53,125. The median family income was \$60,707. The table below represents the median household income for the Village of Elwood as compared to neighboring communities.

Table 6: Income Measures for Elwood and Other Communities

Municipality	Population	Median Household income	Median Family income	Per Capita income
Channahon	7,344	\$71,991.00	\$74,481.00	\$22,867.00
Elwood	1,620	\$53,125.00	\$60,707.00	\$22,442.00
Joliet	77,970	\$47,761.00	\$55,870.00	\$19,390.00
Manhattan	3,330	\$55,559.00	\$62,865.00	\$21,666.00
Rockdale	1,470	\$39,954.00	\$47,232.00	\$18,738.00
Symerton	106	\$60,357.00	\$61,607.00	\$17,863.00
Wilmington	5,134	\$45,659.00	\$53,648.00	\$24,357.00

Sources: U.S. Census Bureau, Census 2000 (1999 Dollars)

The Village's distribution of wealth is shown in Table 7. The Village has an even distribution of wealth with 49.7% of families earning between \$50,000-100,000 and a median family income of \$60,707. The median income is above or equal to most of the neighboring communities. The Village had 22 (4.8%) of families earning less than \$15,000 per year and 18 (3.9%) of those families earned less than \$10,000.

Table 7: Number of Families per Income Group

Income Group	Number of Families	% of Total
less than \$10,000	18	3.9%
\$10,000 to \$14,999	4	0.9%
\$15,000 to \$24,999	31	6.7%
\$25,000 to \$34,999	58	12.5%
\$35,000 to \$49,999	65	14.0%
\$50,000 to \$74,999	132	28.5%
\$75,000 to \$99,999	98	21.2%
\$100,000 to \$149,999	49	10.6%
\$150,000 to \$199,999	6	1.3%
\$200,000 or more	2	0.4%
Total	463	100.0%

Sources: U.S. Census Bureau, Census 2000

Existing Physical Conditions

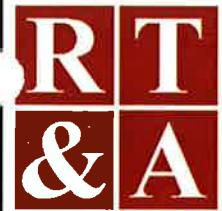
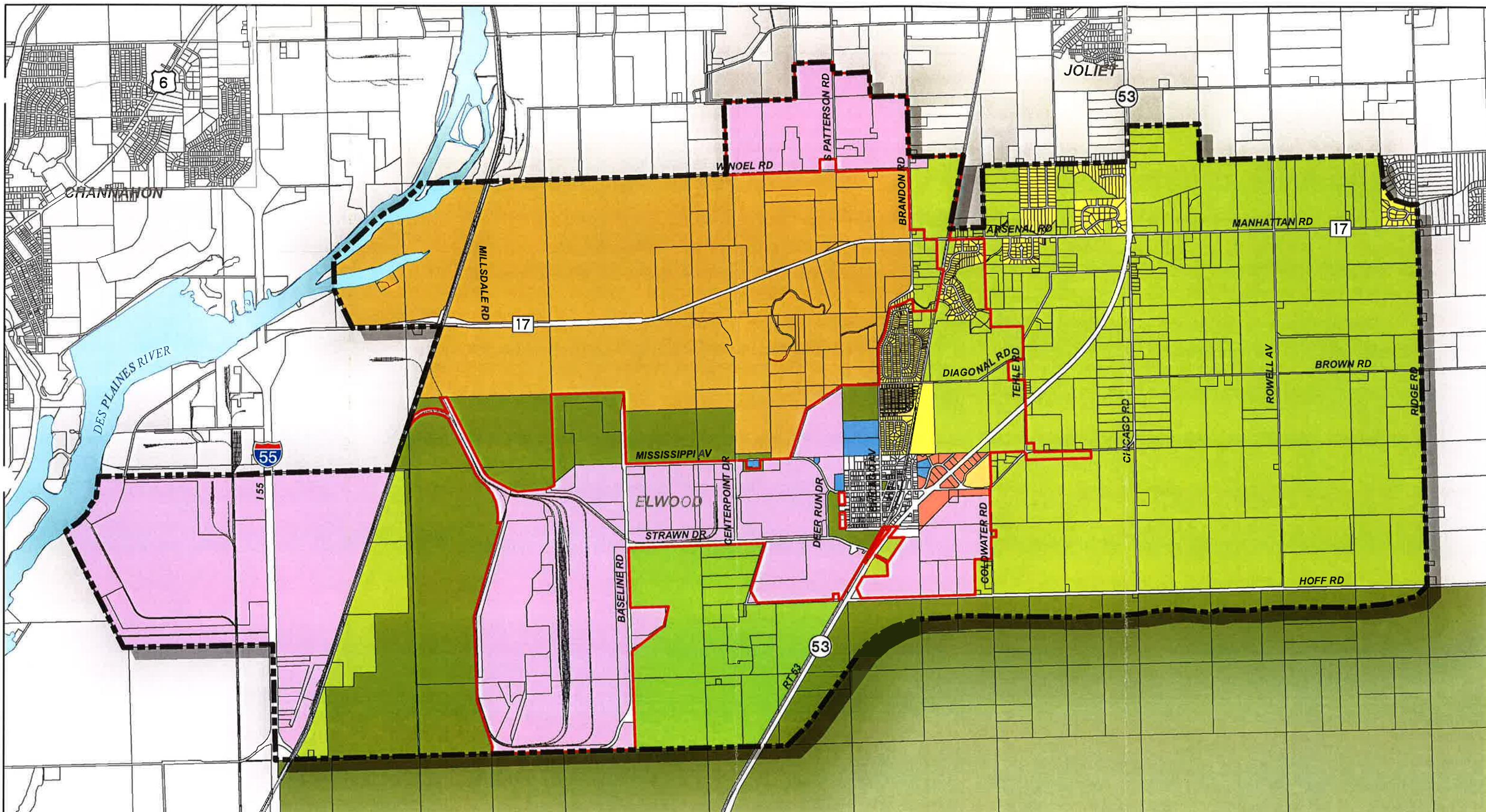
Land Use

The following section describes the existing land use pattern in the Village of Elwood and its surrounding area (Illustration 2). In order to properly determine the pattern of future land uses, the influence of the current pattern of land use and development must be considered.

The total land area within the Village of Elwood is approximately 4,000 acres (~6.25 square miles). Of all developed land within the Village's corporate limits, industrial uses are the most prominent with the development of the Centerpoint Intermodal Center on the former Arsenal and east expansion has added a large amount of industrial capacity to the Village. Residential uses are the second most prevalent and are concentrated around the historic downtown area and north along the east side of the Union Pacific Railway to Arsenal Road. Existing commercial development is limited to Mississippi Avenue. Some light industrial uses are interspersed in the traditional town area.

Land uses surrounding the community varies greatly. An area of heavy industrial use lies west and northwest of the Village along I-55 and the Des Plaines River. Also to the West is the Joliet Army Training Area. The former Joliet Army Ammunition Plant (Arsenal) lies southwest of the Village. Redevelopment of the former arsenal includes the construction of the Centerpoint Intermodal Center, the Abraham Lincoln National Veteran's Cemetery lying southwest of the Village. The remainder of the arsenal property within the Village's planning area is being incorporated into the 19,000 acre Midewin National Tallgrass Prairie and is under the jurisdiction of the National Forest Service. Within the corporate limits of Joliet, north of the Village of Elwood, lie the Route 66 Raceway and the Autobahn Country Club and the recently annexed Centerpoint Intermodal Center Joliet. The remainder of the planning area is predominantly agricultural with pockets of rural residential development interspersed in the landscape.

-Existing Land Uses are shown on Illustration 2: Existing Land Use Map.



Legend

Planning Area

Elwood Corporate Limits

Existing Land Use

- Agricultural/Rural Residential
- Single-Family Residential
- Multi-Family Residential

Industrial

Commercial

Government/Institution

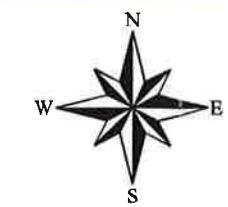
Open Space

Abraham Lincoln National Cemetery

Joliet Army Training Area

Utility

Nearby Municipalities



October 2008

Comprehensive Plan

Existing
Land Use Map
Illustration 2



Village of Elwood

Environmental Conditions and Physical Features

A general environmental assessment was made for the Village of Elwood and its planning area. This provided information on environmental conditions that significantly influence the use of the land. Such an analysis assists in the determination of the appropriate location for various land uses such as residential, industrial, commercial, and open space.

Soils

Using the soil surveys produced by the United States Soil Conservation Service (now the Natural Resource Conservation Service), the soils within the planning area were examined. The capability of these soils for development was considered in this analysis.

Several soil types are found within the planning area in and around Elwood. The most prevalent soil types are the Elliott Silt Loam (146A&B) series, the Morley Silt Loam (194) series, the Varna Silt Loam Series (223) and the Ashkum Silt Loam (232) series. These soils are considered moderately permeable, are gently sloping and somewhat poorly drained soils.

The relatively poor suitability of the predominant soils for construction in the Village of Elwood is a problem that should be considered. The limitations of the soil will not necessarily slow development in the Village of Elwood. However, special care should be taken to avoid situations that could negatively affect the Village. Therefore, during subdivision review, specific attention should be given to developments within areas of poor soils and special care taken to overcome the soil limitations associated with the predominant soils.

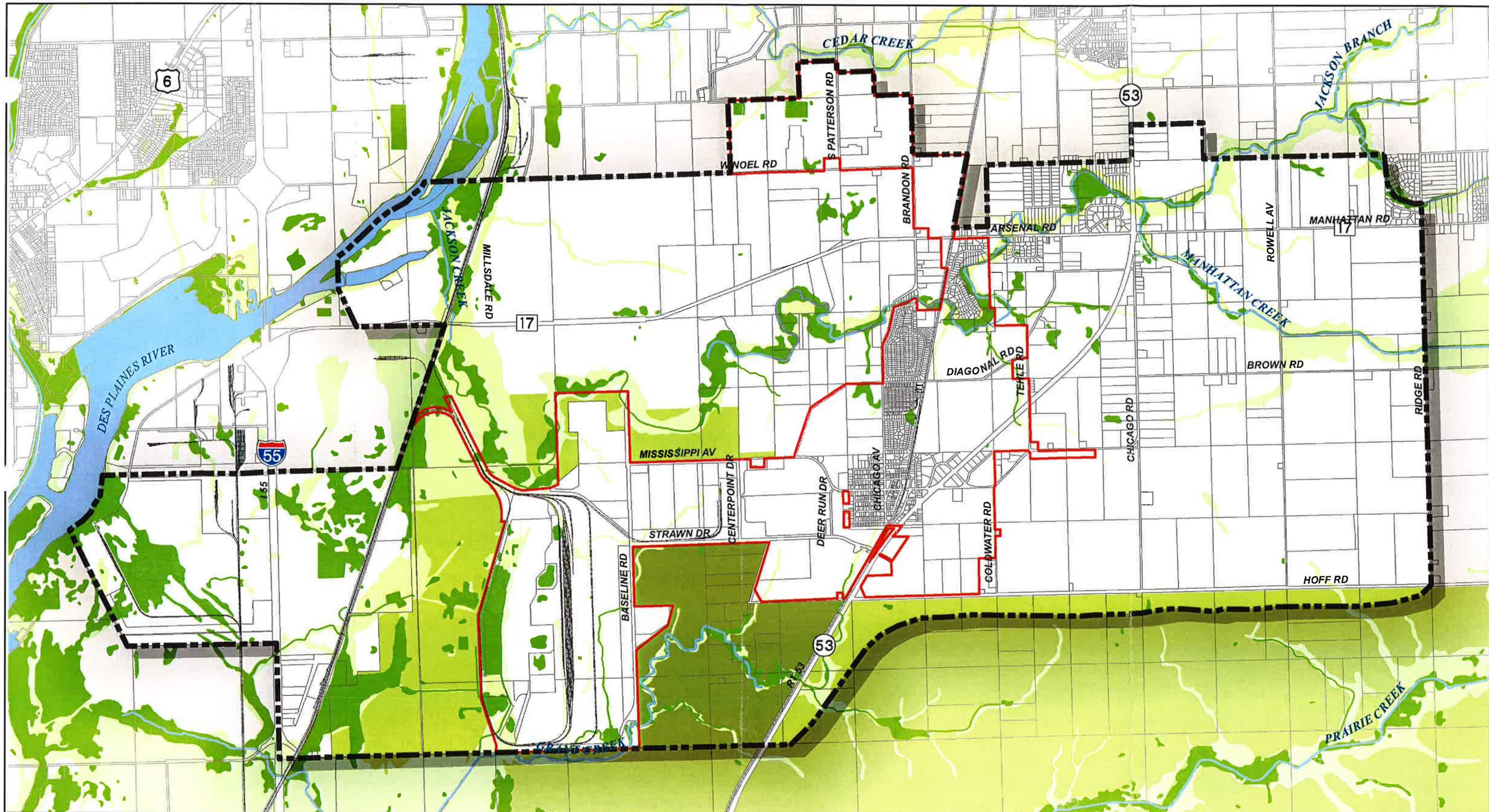
Floodplains

Another environment feature of any community is the floodplain (See Illustration 3). The Illinois Department of Transportation (IDOT) provides the following explanation of a floodplain:

“Rivers and streams are part of nature’s systems for carrying water from high ground to lakes and oceans. Floodplains are the part of the system that nature made for carrying large amounts of water: floods. Flooding is a natural process and floodplains are a vital part of that process.”

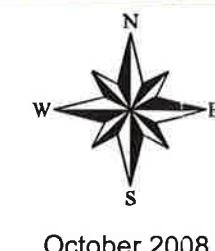
A floodplain is comprised of two elements: the floodway and the flood fringe. In general, the floodway includes a stream’s channel and the immediate area on either side of the channel which facilitates the greatest amount of water movement downstream. The flood fringe is the remaining area of the floodplain where the water rises.

-Mapped floodplain and wetland areas, per aforementioned national maps, can be found for the planning area in Illustration X: Environmentally Sensitive Areas Map.



Legend

Planning Area	Waterway
Elwood Corporate Limits	Midewin National Tall Grass Prairie
	NWI Wetlands
	Abraham Lincoln National Cemetery
	FEMA Floodplain



Comprehensive Plan

Environmentally
Sensitive Areas
Illustration 3



Village of Elwood

Transportation

Transportation has been an important factor in the development of the Village of Elwood. The construction of the old Joliet and Bloomington railroad ultimately was the catalyst for the establishment of the Village. Illinois Route 53 has grown from a dirt trail used by settlers and Native Americans to what was once a portion of the famed Route 66, which served as one of the nation's primary highways until the construction of the interstates. A portion of the original Route 66 still runs through downtown Elwood. Transportation again will be a key factor in the future of Elwood.

Several developments have begun to utilize the existing road networks capacity. The first is to the north with the development of the Route 66 Speedway. Although in Joliet, Illinois Route 53 and Manhattan Road will see significant amounts of traffic generated as a result. The events planned for the Speedway will bring hundreds of thousands of people to the area. The road network in and around the Village will bear a good portion of this traffic.

The second development that has had long-term repercussions on the Village' road network is the redevelopment of the Arsenal. The addition of the Abraham Lincoln National Cemetery will add an estimated 350,000 visitors per year to the area in and around Elwood when finished. The development of the remainder of Centerpoint Intermodal Center, east expansion and are adding significant truck traffic to Interstate 55 and Arsenal Road. This area has seen a dramatic increase in traffic. Arsenal Road has recently been expanded to four lanes to accommodate the additional traffic associated with the intermodal center.

The third development that will have an impact on the Village area will be the proposed Centerpoint Intermodal Center Joliet that proposes an extension of Baseline Road north to connect with Vetter Road for the major access to the development. The development is planned to be larger than the Elwood complex and create more truck traffic to Arsenal Road and Interstate 55.

Finally, the construction of the proposed South Suburban Airport near the Village of Peotone could have a tremendous impact on the Village and its current and future infrastructure.

Road Network

These long-term transportation developments will test the existing road networks in and around the Village of Elwood. The township roadway network is adequate for the current levels of traffic volume. However, traffic volumes will begin to increase in the near future as current development trends continue in the Village and surrounding areas.

Interstates: the Village of Elwood has convenient access to both Interstate 55 (west of the Village) and interstate 80 (north of the Village). Arsenal Road provides access to Interstate 55 while Illinois Route 53 provides access to Interstate 80.

Arterial Roads: Arterial roads are roadways that accommodate interregional trips. The Villages of Elwood's arterial roads include Illinois Route 53 and Arsenal/Manhattan Road.

Minor Arterial/Township/Collector Roads: Township collector roads are generally located along section line roads and serve to move local traffic to local destinations and major arterials. While these roads are not necessarily improved to urban standards, they form the framework of the future collector system. Examples of these types of roads are Mississippi Avenue, Brandon Road and Hoff Road.

Minor Collector Roads: Minor collectors serve to move a very limited amount of local traffic to larger collectors or arterials. Minor collectors are streets within a specific development or serving a limited number of developments, connecting them to other collectors and arterials. In the rural and agricultural areas, minor collector roads tend to be longer in length than in the suburban areas because the adjoining lands produce a lower traffic demand per acre.

Railway

Elwood is not currently connected to Chicago by passenger rail service. Many Village residents work in Chicago or other suburban employment centers served by Metra. The residents of Elwood drive to the Metra Stations located in Joliet serviced by the Rock Island District Line and Heritage Corridor Line or to the station in Manhattan serviced by the Southwest Service Line.

Due to the development of the Centerpoint Intermodal Center, there are several major railways that are within Elwood' planning area and ready to serve the Village' industrial needs. The Union Pacific Railroad Runs directly through downtown Elwood and will soon service portions of the Deer Run Industrial Park, while the Burlington Northern Santa Fe (B.N.S.F.) is located west of town and currently offers service to the Centerpoint Intermodal Center, Mobil Refinery and the Stepan facility.

Bicycles and Pedestrians

Many streets within the Village are without sidewalks. Efforts are underway to mitigate this condition, all new developments should be required to provide pedestrian friendly facilities. This is especially needed in areas near school and parks. As traffic volumes rise, the desire for safe, pedestrian friendly facilities will also increase. The city should strive to keep a pedestrian orientated development a high priority when evaluating new plans.

The Village has a Parks and Open Space Plan that should be referenced as new developments begin their planning stages. The Village and Developers should provide for bicycle paths or lanes to tie residential land uses to schools, recreation areas and commercial developments. Paths can be provided in the greenbelts and open space created by streams, creeks, roadways, pipelines and utility rights-of-way. Portions of the collector street network should also be delineated for exclusive bike lanes.

Community Facilities

Schools

The Village of Elwood is served by Elwood School District 203 and includes Elwood Elementary School, which provides education for children in grades kindergarten through eighth grade. There were a total of 426 children within Elwood School District 203 enrolled in 2007

High school aged children (grades 9 through 12) attend Joliet Central High School.

Fire Protection

The Village of Elwood is served by the Elwood Fire Protection District, which is located on Mississippi Street on the far western edge of the original town limits.

Police Protection

Police protection is provided by the Village of Elwood Police Department, which is located in the Village Hall on Mississippi Avenue. The Will County Sheriff's Department provides police protection outside the Village of Elwood.

Utility Systems

The Village of Elwood is served by public water and sewer services. Wastewater treatment is provided by a treatment plant located on Baseline Road north of Strawn Drive.

Parks and Recreation

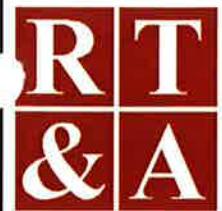
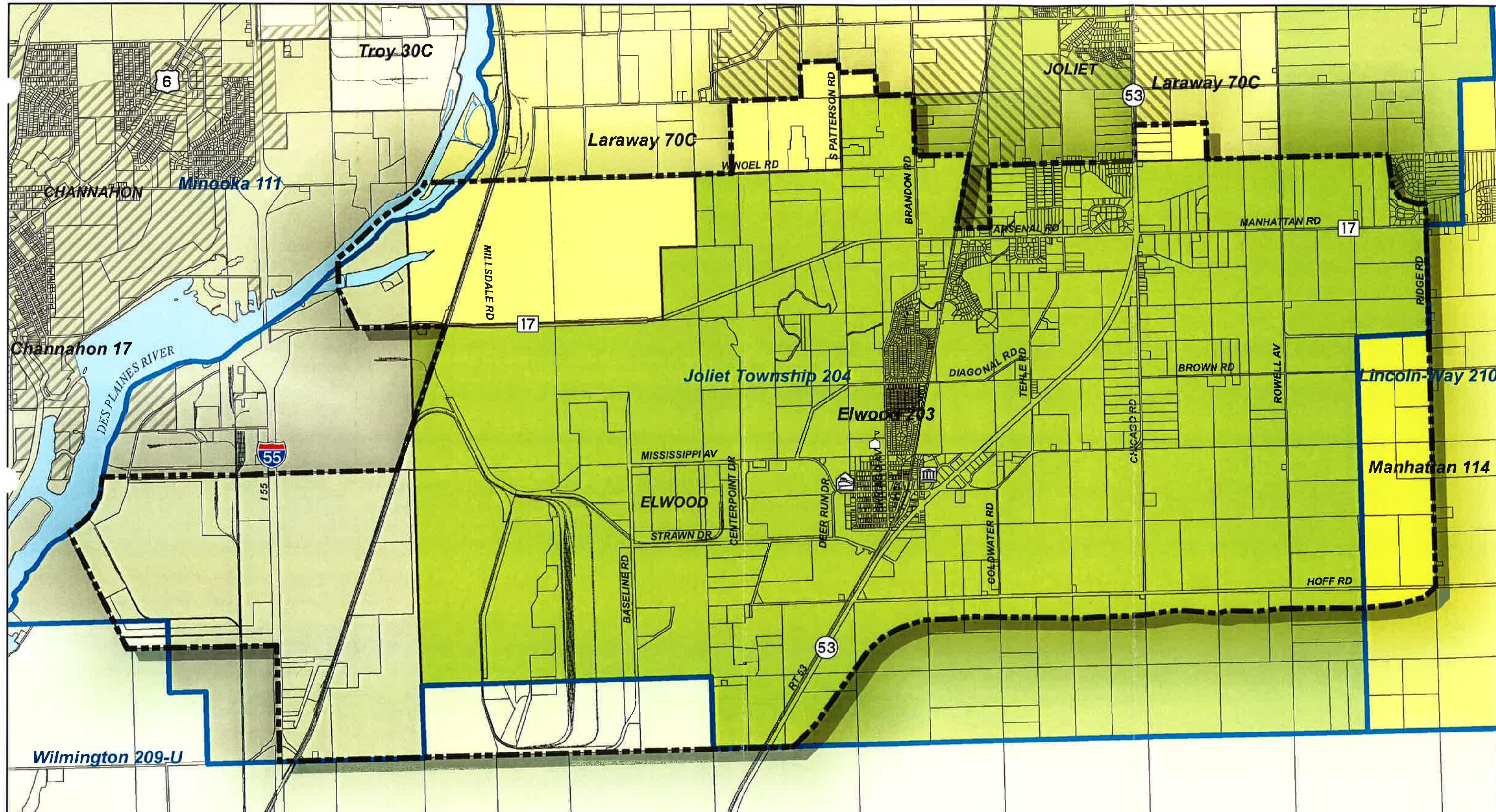
The Village is well served with its existing parks. The Little Park located in the downtown area has served the community since the Village was first established. Little park was established as the "town square" or "village green" when the original town was platted. Little park has served the community for nearly 150 years and is a true amenity to the Village and has recently been renovated with a new gazebo and walking paths.

The Village is also well served by the Erickson regional park located on its northwest side. The park is located on the west side of Brandon Road, north of the elementary school. The park is over 80 acres in size and includes a children's garden and a driving range.

The Centerpoint Park located on the western edge of the town center has been developed for ball fields and a trail connection to the downtown area.

The Village Board has established a Parks Commission to serve as an advisory board and adopted a Parks and Open Space Plan. This group will help in overseeing the provision of parks and recreation that is required in all new development. The Parks and Open Space Plan should be used to protect and enhance natural resources such as woodlands, wetlands, and watercourses. The Parks Commission will aid the Village Board in making this a reality.

*-Mapped district boundaries can be found in **Illustration 4-Jurisdictional Boundaries Map**.*



Legend

Planning Area	Elwood Elementary School District 203
High School District	Laraway Elementary School District 70C
Wilmington Unit School District 209-U	Manhattan Elementary School District 114
Channahon Elementary School District 17	Troy Elementary School District 30C

- Fire Station
- School
- Village Hall

W N E
S
October 2008

Comprehensive Plan
Jurisdictional Boundaries
Illustration 4



OVERALL GOALS AND OBJECTIVES

Goals and objectives are used to provide a framework for the implementation of the Comprehensive Plan. Goals describe the desired results of the Comprehensive Plan's implementation. Objectives are the specific purposes that advance action toward the goal.

The Village of Elwood will achieve these goals and objectives by employing appropriate policies and plans, including the Zoning Ordinance and Subdivision Ordinance. The Comprehensive Plan assists in outlining the process and strategies of implementation for advancing the general goals and more specific policies set forth by the Village of Elwood.

Community Growth and Character

Goal: Maintain a well-balanced Village environment of residential, commercial, industrial, public and open lands that develops within the scope of the Village's plans and services.

Objectives:

- Insure that new development is compatible with existing development.
- Insure that new development is consistent with the goals, policies, and guidelines of this Comprehensive Plan.
- Define community edges and growth limits.
- Continue the establishment of boundary agreements with neighboring communities.
- Annex land planned for inclusion within the Village's ultimate boundaries.
- Establish a strong relationship with County staff in order to continue exercising effective control of the Village's 1 ½ mile planning jurisdiction.
- Prepare and maintain a Village master sewer and water plan that will meet the needs of the Village's ultimate boundaries.
- Ensure that all land uses are effectively screened and buffered from the negative attributes of neighboring or adjacent uses.

Goal: Ensure that new development contributes to and enhances the small town, rural character of the Village.

Objectives:

- Establish guidelines that will foster a rural character in areas of new development.
- Establish specific design guidelines addressing such issues as setbacks, landscaping, impervious coverage and signage along major and minor arterial and collectors, etc.
- Utilize links with the community's past as cultural and design elements in the community's future growth.
- Encourage growth consistent with the density and intensity that provides the Village's present character.
- Ensure that planning becomes a continuous and evolutionary process that engages the Village to strive toward a vision for the community.
- Develop and adopt development regulations that promote community goals through the use of sophisticated and creative design guidelines.

Goal: To maintain, enhance and expand the traditional village downtown.

Objectives:

- Ensure that any new construction or re-development within the limits of the traditional town center is consistent with the character of that district.
- Develop design guidelines and Village controls that allow and encourage traditional town design.
- Enhance the appearance of the traditional village center (specifically Mississippi Street) through streetscaping and building improvements.

Goal: Magnify the role of open space and natural resources in the Village's planning and development actions.

Objectives:

- Utilize open space as a major design component within new developments to maintain the rural character of the Village.
- Preserve natural resources such as woodlands, wetlands, and watercourses from unnecessary disturbance.
- Integrate natural resources into development plans.
- Create connections of neighboring open spaces to allow for accessibility.

Residential Development

Goal: To provide and maintain housing and neighborhoods that reflect the planned character of the Village and meet the Village's goals for the integration of neighborhoods through a hierarchical open space/recreation network.

Objectives:

- Preserve and enhance existing neighborhoods.
- Improve the quality of existing residential areas where necessary and possible through both building improvements and infrastructure improvements (curb, gutter, sidewalks, etc.)
- Establish design, density, and open space criteria for all future development areas.
- Interconnect neighborhoods visually by roads, walkways and open space.
- Adopt appropriate zoning and subdivision controls to ensure high quality residential development that is compatible with the existing character and goals of the Village.
- Establish new residential developments in a node pattern around centralized neighborhood and regional commercial.
- Encourage new residential development to occur in a pattern contiguous to existing neighborhood limits.

- Provide pedestrian oriented transportation options such as bicycle and walking paths in all residential neighborhoods. Paths should be planned to connect with surrounding neighborhoods, open space, points of interest, and commercial areas.
- Ensure that new residential development is compatible with nearby existing housing in terms of character and scale.
- Encourage all residential development to occur in master planned neighborhoods which incorporate creative site planning, creative housing design, open space systems, pedestrian systems and high quality landscape design.
- Provide open space within future development areas that will enhance each neighborhood as well as the Village as a whole.
- Plan for areas of neighborhood commercial to be located for convenience needs of the surrounding housing.

Goal: To provide a variety of housing types and densities which satisfy the varied needs of existing and future residents of varying ages, family size and lifestyles.

Objectives:

- Encourage the provision of housing suitable to a variety of lifestyles, family sizes and income levels.
- Encourage a mixture of housing types and price ranges as part of master planned traditional neighborhoods.
- Discourage the concentration of multi-family residential into a single area that is not an integral part of an overall mixed neighborhood plan.
- Encourage the location of multi-family housing as a component within planned neighborhoods.

Economic and Business Development

Goal: To develop an environmentally compatible industrial and business base that enhances the Village's tax base and provides employment opportunities to Village residents.

Objectives:

- Identify suitable areas for major employment center industries and control neighboring land uses that would reduce the economic marketability of these lands.
- Maximize industrial development potential that is associated with the Village's location along I-55 and the former Arsenal properties.
- Identify truck routes that provide convenient access to industry and minimize conflicts with local traffic.
- Strongly encourage industrial development in established or planned industrial areas.
- Promote the development of industry that exceeds environmental performance standards.
- Encourage business park development in planned business park areas.

Goal: To provide a wide range of service and retail uses within the Village that meet both the neighborhood and regional needs of Village residents and are consistent in location, scale and function with the planned character of the Village.

Objectives:

- Provide commercial developments that are compatible with surrounding neighborhoods and are pedestrian friendly.
- Discourage strip development along arterial routes as this creates an unsightly streetscape that will clutter traffic.
- Create neighborhood nodes to provide daily shopping and services into a centrally located commercial development.
- Encourage regional nodes to provide all the shopping and services of a neighborhood node does integrated in with regional shopping and service developments.
- Provide multi-modal (i.e., walking, biking, auto, etc.) access to neighborhood commercial areas.

- Cluster unified highway commercial development at arterial intersections.
- Minimize entrances and stop lights along arterial routes.
- Incorporate standards that command a high degree of performance standards that encourage uniqueness and creativity in site design, landscaping and architecture promoting the community's image along the Illinois Route 53 commercial corridor.
- Minimize the impact of parking lots and segregated retail stores from dominating the streetscape by concealing parking and integrating in large retail with smaller stores.

Goal: Maximize the “main street” character of Mississippi Street in providing residents and visitors with goods, services and employment opportunities.

Objectives:

- Reinforce and improve the operational conditions of existing commercial areas of this downtown district.
- Adopt development regulations that uniquely deal with the requirements of “main street” businesses.
- Prepare a “Main Street Streetscape Plan” for Mississippi Street that includes plans for connecting the pedestrian system of the Village and the Village’s open space hierarchy.
- Promote that traditional role of the “main street” through development and rehabilitation that respects the district’s traditional character.
- Provide efficient and safe means of access to the “main street” to all Village residents.
- Provide adequate parking that is consistent with the traditional character of a main street.
- Enhance the “curb appeal” of Mississippi Street.

Goal: To provide quality facilities and service to every resident and business within the Village without unnecessary increases in the burden on the Village's property tax base.

Objectives:

- Develop a capital improvement plan that includes the Village's planned infrastructure improvements and land acquisitions.
- Utilize the availability of community services and facilities to guide the location of new development.
- Coordinate Facility Planning Area Expansion with development policies put forth in this comprehensive plan.
- Ensure adequate utilities to uses promoting economic development.
- Provide effective and efficient law enforcement and fire protection to all residents of the Village.
- Ensure that developers assume the expense of infrastructure improvements that outweigh the immediate measurable benefit of the Village.
- Promote growth to occur adjacent to existing services to minimize impact.
- Promote commercial growth to be integrated with residential developments to help balance tax income.
- Ensure that new developments provide adequate lands needed for public purposes.
- Aid the efforts of all social service agencies to provide the necessary services for community residents of all ages.

Open Space and Recreation

Goal: Magnify the role of open space and natural resources in the Village's planning and development actions.

Objectives:

- Utilize open space as a major design component within new developments to maintain the rural character of the Village.
- Create a hierachal open space network comprised of neighborhood parks and village wide lands.
- Establish open space linkages that will create a “green” network between open space and park destinations as well as other important destinations such as public uses and neighborhood shopping.
- Establish equations or ratios that balance the development area of a site with a required provision for open space.
- Utilize utility rights-of-way as open space linkages between parks and other Village destinations.
- Establish neighborhood parks that are appropriately located and sized to be a functional element of the neighborhood they serve.
- Ensure that new parks will be able to have park equipment installed in a timely fashion to increase usability.
- Use neighborhood parks and open space as the focal point of new neighborhood designs.
- Link destination points such as parks, commercial and schools with pedestrian and bike paths.

Goal: Maintain and protect natural resources located in the Village's planning area.

Objectives:

- Preserve natural resources such as woodlands, wetlands, and watercourses from unnecessary disturbance.
- Integrate natural resources into development plans.
- Preserve the Jackson Creek Corridor as a major element of the Village's open space system.

Transportation

Goal: To develop and maintain a balanced transportation system which provides for the safe and efficient movement of people and goods by all modes of transport.

Objectives:

- To integrate the transportation system plans of the Village, the County and the State.
- Continue to develop a transportation hierarchy that relates to the Village's land use policies.
- Preserve rights-of-way necessary for future elements of the roadway system.
- Ensure that site designs are such that the functions of adjacent roadways are not impeded.
- Encourage the development public transportation service to and within the Village.
- Ensure adequate access to I-55 for all Village residents and businesses.
- Separate business and industrial traffic from residential traffic.
- Reduce the possibilities for conflicts between various types of traffic and modes of transportation.
- Promote linkages between village destinations via bicycle and pedestrian systems.

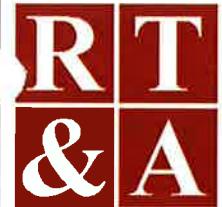
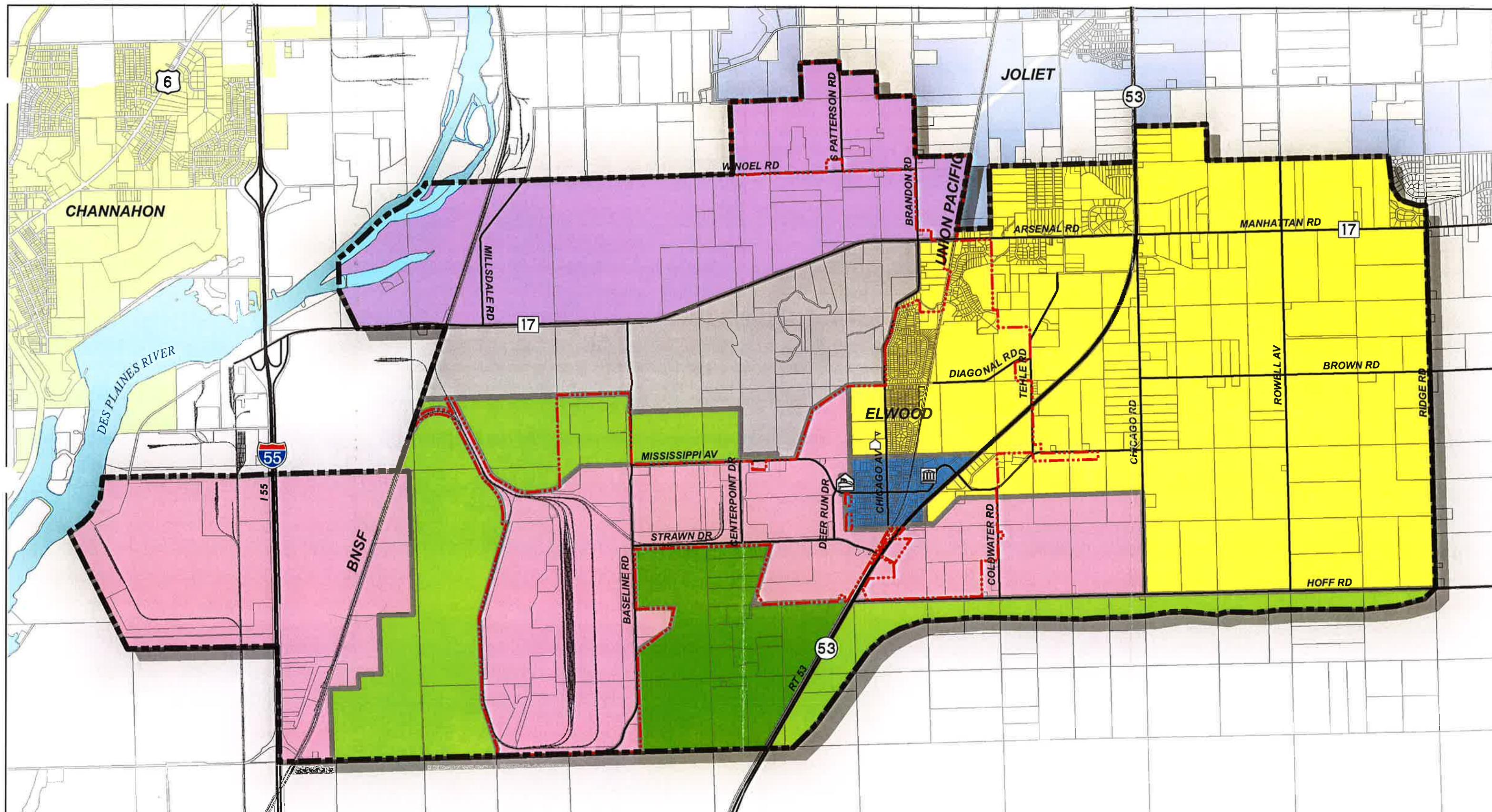
LAND USE PLAN

The land use plan for the Village of Elwood is broken into two components designed to cooperatively achieve the overall goals of the community. The first component is a set of planning sectors which establishes general design trends to be followed within specified areas. The second component is the land uses which give fundamental guidelines to where different uses and intensities should be planned within the Village's planning boundary. All developments should follow the Village design guidelines and development ordinances.

Planning Sectors

The land use for the Village of Elwood can be separated into four general sectors. These sectors give general guidelines as to what types of uses and intensities can be found within them.

*-Planning Sectors can be found in **Illustration 5- Planning Sectors Map**.*



Legend

	Planning Area		Southwest Industrial Area
	Elwood Corporate Limits		Traditional Town
	City of Joliet		Joliet Army Training Area
	Village of Channahon		Suburban
			Abraham Lincoln National Cemetery
			Northwest Industrial
			Midewin National Tallgrass Prairie

Comprehensive Plan



Planning Sectors

Illustration 5

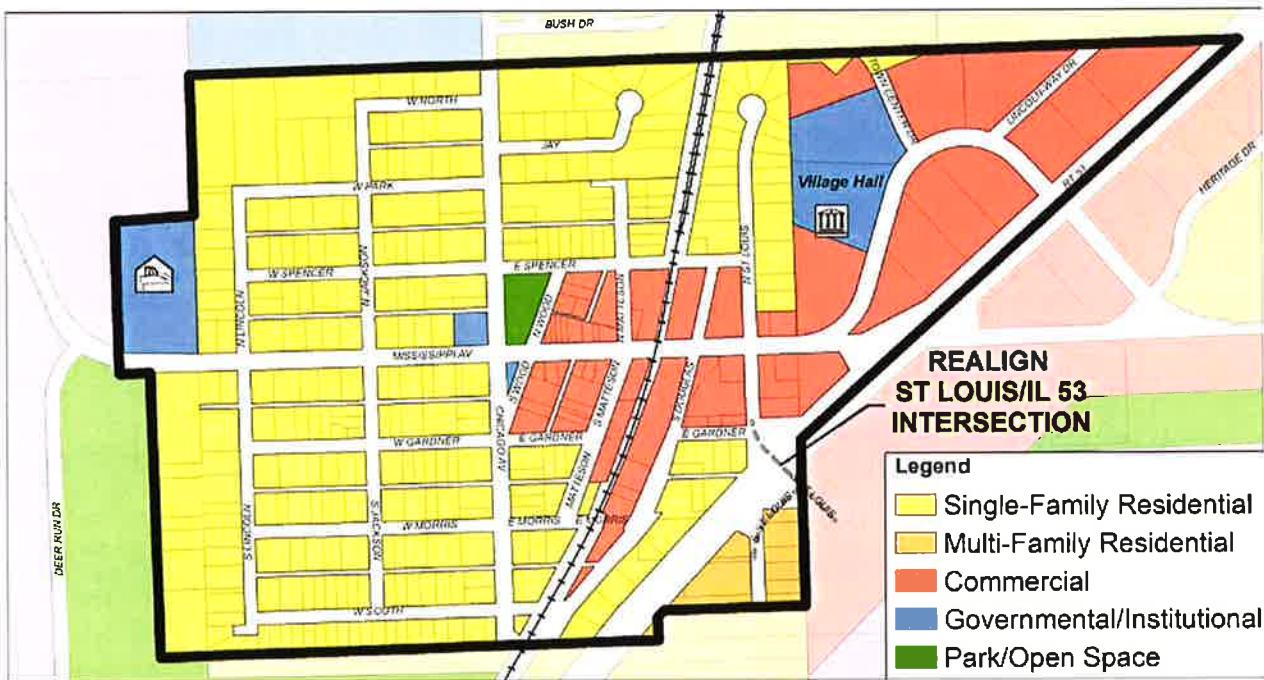


Village of Elwood

Traditional Town Sector

The proposed land use plan for the Traditional Town Sector envisions the continued traditional neighborhood function of this sector anchored by the restoration of the Village's "main street". The plan for this sector calls for central business district commercial along Mississippi Avenue anchored by the Village Square and the Village Hall. The remainder of the sector shall be comprised of predominantly traditional, single-family residential homes.

Figure 4: Traditional Town Sector



Planning and Design Guidelines

- Preserve the historical, traditional neighborhood character and function of the sector.
- Any development or redevelopment in the sector should be at a human scale oriented toward pedestrian activity.
- Site and structure requirements in this sector should strengthen the traditional neighborhood character by requiring smaller front yard setbacks and narrower traditional neighborhood lots.
- Buildings along Mississippi Avenue should be two story mixed-use buildings to be designed in the traditional mainstreet character.
- Courtyards should be utilized to tie into public parking areas.

- Architectural and landscaping elements should be provided that are consistent with the historic and traditional character of the neighborhood - this includes residential, commercial and institutional uses.
- Sidewalks should be established throughout the sector to strengthen the pedestrian connection of the residences to the main street.
- The Village square should be redeveloped to provide a civic square function to the neighborhood in addition to its current recreational function.
- Buildings should be near the street with pedestrian access and parking lots within this sector should be located behind buildings.

Suburban Development Sector

The land use plan for the suburban Corridor Sector calls for a mix of typical suburban uses and intensities. Specifically, the plan designates areas along Route 53 for arterial commercial uses. These community and highway commercial uses are generally shown in the plan to be buffered from single-family areas by areas of higher density residential use. The remainder of the sector is planned for detached single-family residential use.

Planning and Design Guidelines

- Commercial developments along Route 53 are intended to provide sites for both highway oriented commercial uses and community shopping uses.
- Commercial areas should be concentrated within the major intersections and have limited curb cuts onto both Route 53 and intersecting collectors.
- Shared entrances, frontage roads, and cross access points should be provided throughout the arterial commercial areas.
- Appropriate setbacks, screening and landscaping should be provided on all commercial properties on both the arterial highway side and the adjoining residential sides.
- Commercial developments should be accessible to the surrounding residential.
- Special attention should be given to the architectural and landscape appearance of all developments along Route 53 as it serves as the gateway to the Village.
- Green spaces should be placed along arterials to maintain an attractive streetscape.
- Residential development plans for sites with environmentally sensitive areas should not only preserve such areas but also utilize them creatively in the design process.

- Multi-family residential areas should be developed as Planned Unit Developments to promote design excellence and integrity to these highly visible properties.
- Multi-family areas should be comprised mainly of attached single-family dwellings not exceeding a gross density of six (6) dwelling units per acre.
- Detached Single-family developments should be planned in conformance with the Village's zoning requirements.
- Detached single-family developments should not exceed a gross density of three (3) dwelling units per acre.
- All residential developments shall provide residents with access to neighborhood parks.
- All developments within the Suburban Corridor Sector shall provide open space, trail, and roadway linkages to other properties and destinations.

Northwest Industrial Sector

The land use plan for the Northwest Industrial Sector calls for Industrial uses north of Arsenal Road and west of the Union Pacific Railroad. The Village anticipates the Joliet Army Training Area being reduced to allow for development of additional parcels north of Arsenal Road. The industrial uses should be significantly buffered from non-industrial uses to provide a separation from the residents of Elwood.

Planning and Design Guidelines

- The Industrial designation allows for traditional manufacturing, distribution and warehouse uses that are to be operated in a clean and non-obtrusive manner and are substantially buffered from existing and planned residential uses in the Village.
- High levels of screening and buffering should be provided for all properties in this sector that abut non-industrial property.
- This sector is a key component in the economic development goals of the Village.
- Transportation access for truck and employee traffic should be provided in such a manner as to not conflict with the daily function of the remainder of the Village.

Southwest Industrial Sector

The land use plan for the Southwest Industrial Sector calls for predominantly Industrial uses with an area of Light Industrial serving as a buffer to the residential neighborhoods of Elwood. This sector of the planning area includes some of the existing heavy industrial uses along I-55 and the DesPlaines River as well as the Centerpoint Intermodal Center Elwood and East expansion as well as additional areas to Chicago Road along Hoff Road. The areas adjacent to residential and non-industrial uses should provide significant buffers and berthing.

Planning and Design Guidelines

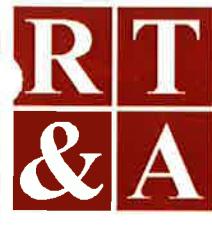
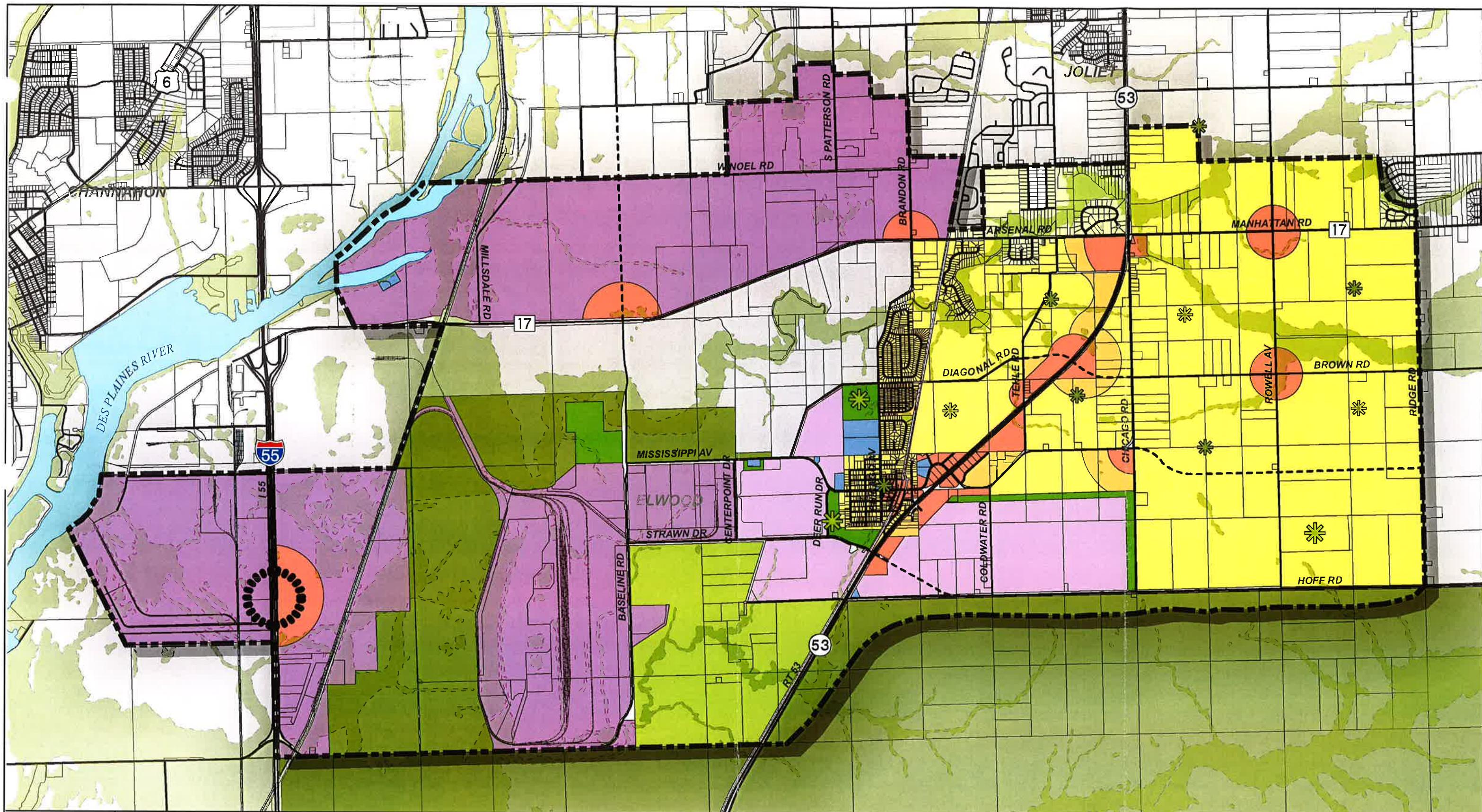
- The Industrial designation allows for traditional manufacturing, distribution and warehouse uses that are to be operated in a clean and non-obtrusive manner and are substantially buffered from existing and planned residential uses in the Village.
- Light Industrial uses are characterized as clean and non-obtrusive uses conducted wholly in enclosed facilities with limited outward signs of industrial usage.
- Light Industrial shall have a high level of design quality.
- Light Industrial developments should be developed in a business park or campus environment.
- High levels of screening and buffering should be provided for all properties in this sector that abut non-industrial properties.
- Transportation access for truck and employee traffic should be provided in such a manner as to not conflict with the daily function of the remainder of the Village. Trucks should utilize the Industrial Traffic Route.
- This sector is a key component in the economic development goals of the Village.
- A green buffer is shown on the land use plan to show where significant buffering will be required adjacent to proposed non-industrial uses within the Village. The buffering should provide a separation between the industrial uses and the residents of Elwood.

Land Uses

The following section describes the desired future land use patterns in the Planning Area. The Land Use Plan is intended to serve as a guide for making land use decisions for the Village. While the physical plan depicts possible scenarios for development, it is not meant to be an inflexible tool for land use control.

The land use plan implements areas beyond the municipal limits to help ascertain the proper and expected growth of the Village and provides the foundation to make land use decisions. The following land uses have been developed to better understand the current and future needs of the community. Development within each land use should follow the Village guidelines and ordinances to provide for cohesively designed neighborhoods and compatibility of neighboring uses.

*-Future land use areas can be found in **Illustration 6- Proposed Land Use Map**.*



Legend

Planning Area
 Proposed Interchange
 Existing Road
 Proposed Road

Proposed Land Use

Commercial
 Rural Residential
 Single-Family Residential
 Multi-Family Residential

Light Industrial
 Industrial

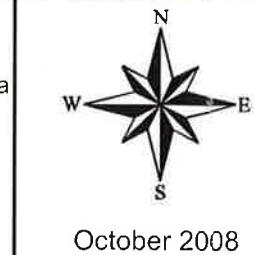
Governmental/Institutional

Park/Open Space
 Midewin National Tall Grass Prairie
 Abraham Lincoln National Cemetery
 Joliet Army Training Area

Environmentally Sensitive Area

Parks Plan

Community Park
 Neighborhood Park



Comprehensive Plan

Proposed Land Use Map
Illustration 6



Rural Residential

In order to form a needed transition between residential and agricultural uses and avoid conflicts between them, a low density, large-lot area has been established. The permitted lots in this area will be no less than 20,000 square feet in size and will maximize the uses of open space. Minimization of site disturbance and visual impact through preservation of existing natural features such as woodlands, flood plains and wetlands is encouraged.

Single-Family Residential

This land use allows approximately 2 to 3 lots per acre. Developments within this area should utilize the Village of Elwood Residential Design Guidelines. Open spaces should be provided in the form of neighborhood parks and the preservation of natural resource areas. Development in this area should occur contiguously to existing developments. Developments should be designed to connect and blend cohesively with one another. All residential developments should create design techniques that discourage monotony, creating neighborhoods of distinctive design and individual character. Residential developments should provide site plans that place a strong emphasis on the relationship between streets, buildings, open spaces, entry features and landscaping. Following are recommended guidelines for the preparation of residential site plans.

Multi-Family Residential

This land use allows densities between 3 and 6 units per acre. Developments within this area should utilize the Village of Elwood Residential Design Guidelines. These residential uses are to act as a transition between standard residential and commercial/industrial uses. The residential uses should be incorporated with a traditional commercial sector which will provide services for the residents. Ideally, the commercial uses will be developed as pedestrian friendly corridors which help develop a traditional pattern of residential, commercial and open space.

Ideally, the commercial/residential transition could develop to promote; a human scale of development oriented toward pedestrian activity; an integration of commercial, residential and recreational uses that provide its residents with a variety of housing opportunities, workplaces, shopping, services, public facilities and recreational amenities; development that preserves and enhances the neighborhood's natural features and resources in the neighborhood's park and recreation system; a rectilinear street and block pattern designed with sensitivity to the natural landscape; urban design standards that allow for the compatibility of buildings and other neighborhood features as determined by their arrangement, bulk, form, character and landscaping that aid in establishing livable communities; architecture and landscaping, civic buildings, town squares, village greens, and other public facilities that create a sense of place and community identity, enhancing the Village's character; and an attractive streetscape that is oriented toward pedestrian activity but encourages the safe and efficient use of the automobile.

Commercial

Developments within this area should utilize the Village of Elwood Commercial & Retail Design Guidelines. The commercial area is designated to provide the services needed for the local and regional residents. The local services can be provided nearby or within residential developments. These commercial uses may be small with little impact to the residential uses. However, to increase the tax base of the community, some commercial uses must be developed which serve the regional customer. These uses are usually larger scale developments which have a larger impact on residents, traffic and public services. Location of such activities should be designed to be compatible with neighboring residential development. Larger scale commercial developments should occur on major transportation routes. Local and neighborhood commercial activities should occur on secondary routes or at strategic node locations.

Light Industrial

Industrial uses must be provided for to increase the tax base of Elwood. These uses must be developed with buffering from residential communities to protect the welfare and safety of such areas. The Light Industrial Districts will provide for less intense industrial uses and provide a buffer between commercial uses and residential uses and the intense industrial/manufacturing uses.

Industrial sites should use the following standards for good site planning, landscaping and architectural design that will attract businesses that share the desire to conduct business in a community with high standards. Properly planned industrial development will fit in with surrounding land uses and create a strong economic and employment base for the Village. Buildings should be oriented so the office spaces with higher levels of architectural detail are facing the public right of way. The scale of the building is derived from the physical size, massing and also from the apparent scale as determined by the visual appearance of the building. Architectural and site features should be used to create the appropriately scaled buildings for the site. Buffering shall be appropriately scaled for the intensity of the proposed use and those of adjacent uses and should provide for year round screening.

Industrial

This area provides for more intense industrial and manufacturing uses to be allowed within the Village. Uses in this area should have easy access to Arsenal Road and I-55. Large areas should be planned developments to provide for unified design, easy access and utility placement as well as cohesive buffering and screening plans.

Park/Open Space

This designation includes areas for use as permanent open space for either passive or recreational uses. When possible, this area should be incorporated with environmentally sensitive areas. Open space and recreational amenities should be included in all residential and commercial projects in the form of greenways/greenbelts and should, where possible, include recreational trails to further promote safe and pedestrian friendly interaction of diverse land uses.

Parks can be designed to meet the needs of the immediate neighborhood in addition to the greater community. Recreational or bike trails can also be located along the rights-of-way of arterial and collector roads. Such a system of greenways and bike trails encourages active recreation, decreases reliance on the automobile and provides an additional measure of safety to pedestrians and bicyclists.

Proposed park locations shown on the Proposed Land Use Map are from the Village of Elwood Parks and Open Space Plan. Neighborhood Parks are generally less than ten (10) acres in size and are located for convenient pedestrian access from adjacent residential areas. Community Parks are designed to serve a group of neighborhoods or geographic areas defined by major traffic ways.

TRANSPORTATION PLAN

To date, the Village of Elwood has been very fortunate not to experience the traffic problems that plague many rapidly growing communities. However, as the Village continues to develop, proper attention should be given to the planning of the transportation networks that will serve the expanding population. There are several important classifications of roads in the Village's Subdivision Ordinance that primarily compose the transportation plan. These roads are collector thoroughfares, secondary thoroughfares, major thoroughfares and interstate. The characteristics and standards of design of these streets are found below (Table 8).

Table 8: Hierarchy of Functional Road Classification

Road Class	Minimum R-O-W	Minimum Pavement Width	Traffic Volume (ADT)*	Desired Level of Service
Collector Thoroughfare	80'	37'	5,000-15,000	C**
Secondary Thoroughfare	80'-100'	40' + Median	15,000+	C**
Major Thoroughfare	100'-200'	State	15,000+	C**

*Annual Average Daily Traffic - The total volume passing a point or segment of a highway facility in both directions for one year divided by the number of days in a year.

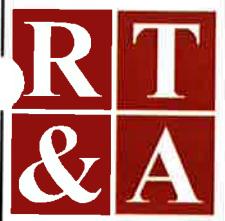
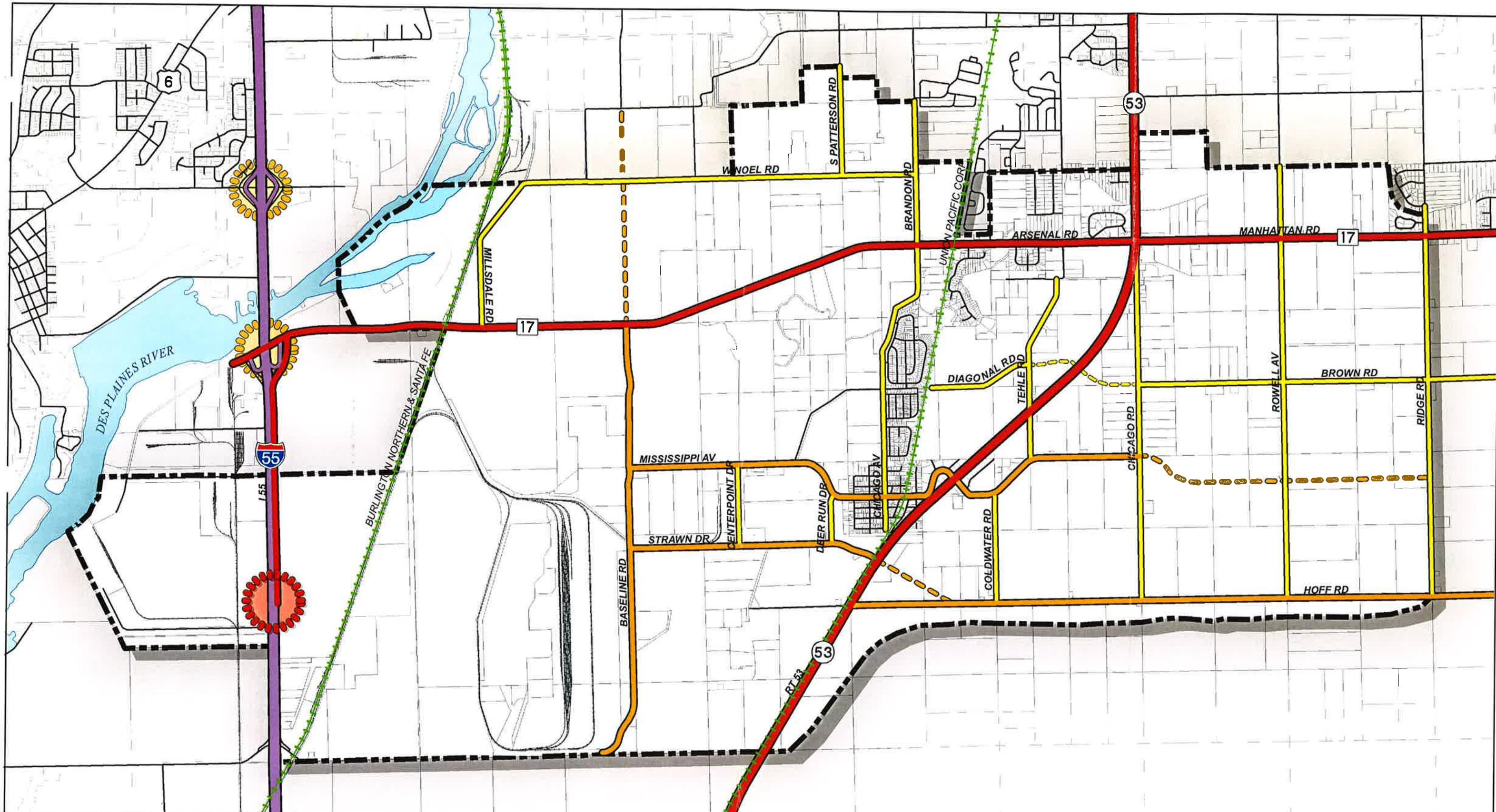
**Level of Service (LOS) is a measure of traffic density that quantifies the proximity of vehicles to each other within the traffic stream and indicates the degree of maneuverability within the traffic stream. The LOS for a level C classification is characterized by an average vehicle spacing of 189 feet for a maximum density of 28 passenger cars per mile per lane. An LOS of A allows a car to travel freely at the speed limit without any delays. An LOS of F is the equivalent of traveling in a crowded parking lot.

Transportation Classifications

Interstate Highway

The planning and maintenance of the interstate is the responsibility of IDOT. Interstate Highway 55 runs north-south through the western portion of the planning area. This highway provides transportation links for the bulk of regional industrial and commercial traffic, and provides easy access to and from the Village of Elwood. The presence of Interstate 55 is a valuable asset for the future development of Elwood economically and culturally. There is a plan to relocate and realign the Arsenal Road interchange farther south to provide for more efficient and safer access to I-55 for the increased car and truck traffic seen with the residential and industrial growth.

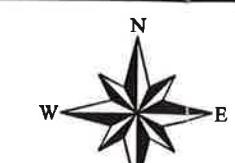
-Proposed road conditions can be found in Illustration 7-Proposed Transportation Plan.



Legend

The legend is organized into three main sections: Planning Area, Road Types, and Railroad Types.

- Planning Area:**
 - Planning Area: Represented by a black square icon.
 - Existing Interchange: Represented by a yellow circle icon with a brown border.
 - Proposed Interchange: Represented by a red circle icon with a brown border.
- Road Types:**
 - Interstate: Purple solid line.
 - Major Thoroughfare: Red solid line.
 - Secondary Thoroughfare: Brown solid line.
 - Collector Thoroughfare: Yellow solid line.
 - Local Street: Black solid line.
 - Existing Road: Black solid line.
 - Proposed Road: Black dashed line.
- Railroad Types:**
 - Class I Railroad: Green dashed line with a cross-hatch pattern.
 - Other Railroad: Black dotted line.



October 2008

Comprehensive Plan

Proposed Transportation Plan



Village of Elwood

Major Thoroughfare

The plan outlines two major thoroughfares for longer-term development and upgrade. Major thoroughfares in the Village of Elwood should ultimately be designed to handle in excess of 15,000 vehicle trips per day. These roads provide regional access to neighboring communities and frequently have access to interstate highways. Overall access to these roads should be greatly limited. Under the plan, Illinois Route 53 serves as a major north-south thoroughfare for the Village and Arsenal/Manhattan Road serves as a major east-west thoroughfare.

Major Thoroughfare Planning Guidelines

- Greatly limit the number of individual lots which load directly onto major collectors.
- Minimize commercial strip development by creating neighborhood and regional nodes.
- Use shared entrances, frontage roads and cross-access connections between properties along Route 53 and Arsenal Road to limit the number of access points.
- New intersections should be perpendicular to existing Major Thoroughfares.
- Provide signalization at road intersections as warranted by demand.
- Place restrictions on engine braking near existing and proposed residential sites.
- Obtain necessary right-of-way during annexation and development review processes.
- Establish screening and buffering areas along the rears of lots abutting Major Thoroughfares. Such screening should be both effective for the adjoining uses, as well as add aesthetic benefit to the character of the corridor.

Secondary Thoroughfare

Secondary thoroughfares are designed to handle traffic flows in excess of 15,000 vehicle trips per day. A typical cross section would include five or more lanes of traffic. These roads provide access and circulation for all land uses within the Village. Secondary thoroughfares are not intended to serve regional travel, but may connect adjacent municipalities on a limited basis. Baseline Road, Hoff Road, Strawn Drive, and Mississippi Avenue are designated as secondary thoroughfares in the plan to serve as sub-regional connectors to major destination point in the Village's planning area.

Secondary Thoroughfare Planning Guidelines

- Greatly limit the number of individual lots which load directly onto major collectors.
- Incorporate pedestrian and bicycle facilities into major collector rights-of-way, as

these rights-of-way serve as the primary connection between the Village's neighborhood units and most destination uses.

- Establish high impact streetscaping along major collector corridors through the use of landscaping, street lighting, and other hardscape and softscape features.
- Establish screening and buffering areas along the rears of lots abutting Secondary Thoroughfares. Such screening should be both effective for the adjoining uses, as well as add aesthetic benefit to the character of the corridor.
- Obtain necessary right-of-way during annexation and development review processes.

Collector Thoroughfare

Collector thoroughfares are designed to accommodate between 5,000 and 15,000 vehicle trips per day. Typically, the design of collector thoroughfares requires a minimum right-of-way width of 80 feet. These streets collect local neighborhood street traffic and connect to the greater arterial system. These streets require a high degree of mobility, as a result, driveway access should be prohibited whenever possible. Proposed collector thoroughfares include Brandon Road, Brown Road, Chicago Road, Chicago Avenue, Centerpoint Drive, Coldwater Road, Deer Run Drive, Diagonal Road, Millsdale Road, Noel Road, Patterson Road, Ridge Road, Rowell Road, and Tehle Road other section line roads of limited length and the proposed frontage road and quarter section line roads.

Collector Thoroughfare Planning Guidelines

- The amount of lots directly loading onto minor collectors should be limited.
- Pedestrian and bicycle facilities should be incorporated into minor collector rights-of-way when such opportunities are present.
- The application of necessary landscaping, street lighting and hardscape/softscape features shall be pursued in the design and development of collectors.
- When the rear residential lots lay adjacent to Collector Thoroughfares, an area dedicated to appropriate screening/buffering shall be allotted to reduce the visual impact of homes within these corridors.
- Obtain necessary right-of-way during annexation and development review processes.
- Develop a combination of interconnected local streets, parking lots to service commercial development within node developments to limit curb cuts along Route 53.

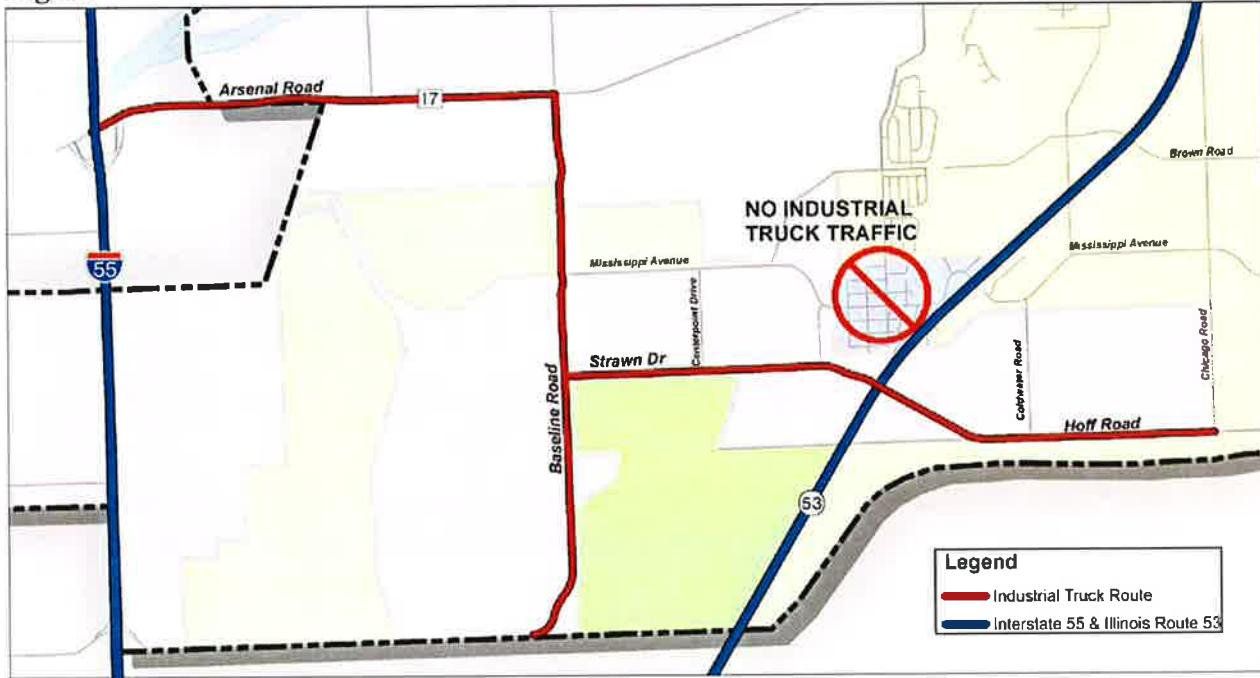
Local Streets

Local streets primarily serve as access points for residential and business uses and should channel traffic to connect with collectors and secondary/major thoroughfares. Trucks should be limited from local streets to delivery traffic only. The St. Louis Street and Illinois Route 53 intersection is planned to be realigned to provide a perpendicular access location.

Industrial Traffic Route

Industrial truck traffic shall be separated from the residential and local traffic within the Village by utilizing industrial traffic routes. The truck route to access I-55 is to follow Hoff Road to Strawn Drive to Baseline Road to Arsenal Road to I-55. Trucks shall not utilize Mississippi Avenue or other downtown roadways except for local deliveries.

Figure 5: Industrial Traffic Route



IMPLEMENTATION

This section of the Village of Elwood Comprehensive Plan has been developed for two purposes:

- To provide insights into the ongoing process of decision-making using this plan; and
- To provide suggestions of “next steps” that the Village can take to bring the vision of this plan to reality.

The majority of the land within the plan is vacant. However, as has been seen with recent annexations and development proposals, much of the land is ripe for development. In any instance, it will likely take 20 years or more for this area to reach its full development potential. During this period, this comprehensive plan should be viewed as a flexible, dynamic document that evolves and adapts to new issues that may face the Village in the future.

Maintaining the Plan

As was discussed earlier, this plan should be viewed as a guideline to the future development of the Village of Elwood, based on an analysis of where the Village is today. Conditions in the Village are dynamic, that is, they are constantly changing. Changes in these conditions may involve land use changes in the Village or public policy changes that arise out of a turnover in the Plan Commission or Village Board.

Proposals that may alter the Village’s long-term development pattern should be weighed carefully. Modifications to the plans should not be made arbitrarily. To ensure that future changes to the plan are appropriate, the following criteria for evaluation should be considered:

- The proposed amendment promotes the goals and objectives of the plan;
- The amendment does not adversely impact surrounding uses in the area or damage the viability of the proposed future use of surrounding properties;
- The amendment is warranted if data used as the basis for the formulation of the plan was erroneous or outdated;
- The amendment is warranted because new issues or needs have arisen that are not addressed satisfactorily in this Plan.

In any instance, the Comprehensive Plan should come under review from time to time. During rapid growth cycles, the Plan should be reviewed annually with necessary modifications being made at that time. In slow to static development periods, the annual review may be shelved in favor of a two to three year review.

Policy Recommendations

This section identifies the “next steps” that the Village should take in an effort to facilitate the implementation of this Comprehensive Plan. It is of vital importance to achieve consistency between the vision of the plan and the regulatory and administrative tools that actually control development. The following implementation policies are recommended for achieving the goals and objectives of this Plan:

- Develop an annexation policy that controls the development of lands considered in this Plan
- Maintain boundary agreements with neighboring communities
- Expand the Village’s FPA boundaries as necessary to achieve development goals
- Develop and maintain a Master Sanitary and Water Plan as a tool for growth management and efficiency
- Update and maintain the Village Zoning Ordinance so that districts are consistent with the goals of the Comprehensive Plan.
- Update the Subdivision Ordinance to ensure consistency with the goals of the Comprehensive Plan
- Develop a Landscape Ordinance
- Develop a stand alone Transportation Plan that provides a broader analysis and future vision that is not necessarily achieved in a Comprehensive Plan
- Encourage the use of Planned Unit Developments to achieve more creative designs of new developments
- Encourage commercial sites to be planned in unison with residential to provide for more cohesive developments.
- Update the Sign Regulations
- Develop programs aimed at enhancing the downtown area and spurring redevelopment of the central business area
- Develop a detailed urban design and streetscape plan for the Central Business District
- Work with the school districts to develop a school facilities master plan that serves the planned populations of Elwood and aids the Village and School District in site selection

- Prepare a Capital Improvement Program for needed improvements such as streets, sidewalks, and any necessary expansions to the sewer and water treatment facilities
- Prepare a detailed Bike and Pedestrian System Plan
- Prepare a Gateway Plan with design guidelines for arterial entrances to the community
- Work with the Fire Protection District to prepare a Master Fire Protection Plan

